



# CONTRACT DRAWINGS FOR THE CONSTRUCTION OF

# COGAN AVENUE RECONSTRUCTION

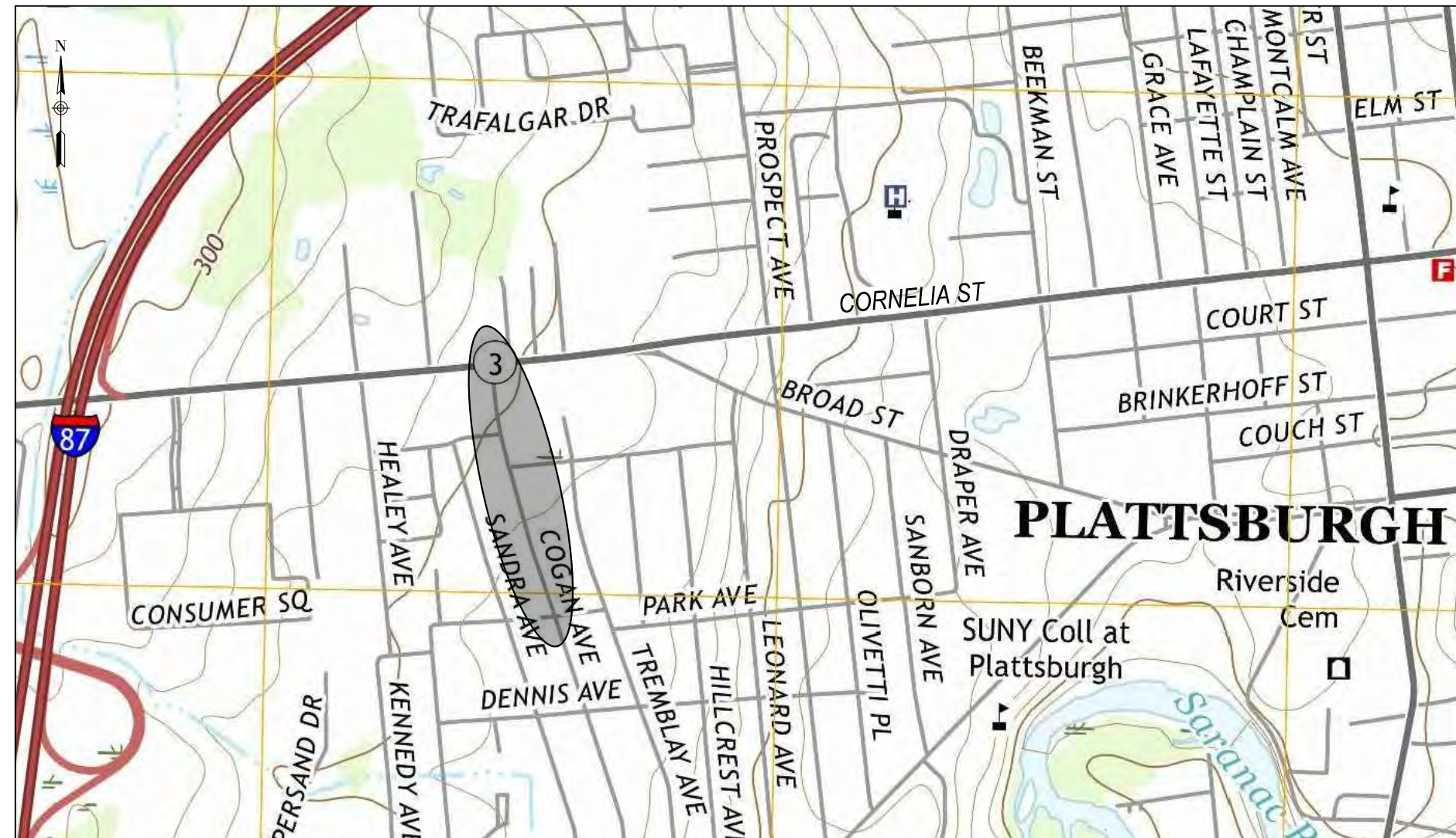
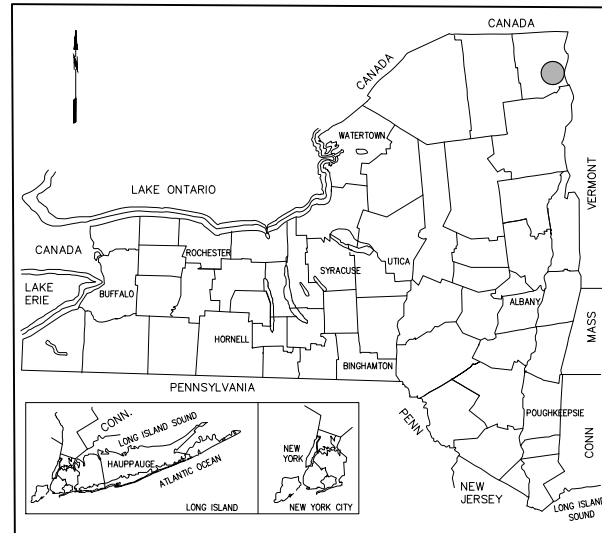
## CITY OF PLATTSBURGH



**Plattsburgh**  
NEW YORK

MARCH 2021

CITY PROJECT NUMBER: 2021-001  
C&S PROJECT NUMBER: A54.003.001



NO ALTERATION ON THESE DRAWINGS IS ALLOWED  
UNLESS SAID ALTERATIONS ARE MADE UNDER THE  
DIRECTION OF A LICENSED PROFESSIONAL ENGINEER.  
IF AN ITEM BOUND BY THE SEAL OF AN ENGINEER  
IS ALTERED, THE ALTERING ENGINEER SHALL AFFIX TO  
THE ITEM HIS SEAL AND THE NOTATION "ALTERED BY"  
FOLLOWED BY HIS SIGNATURE AND THE SPECIFIC  
DESCRIPTION OF SUCH ALTERATION.

**CVR-01**

Mar 04, 2021 - 08:29am  
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ALIGNMENT	
ABBR.	DESCRIPTION
AD	ALGEBRAIC DIFFERENCE
AH	AHEAD
AZ	AZIMUTH
BK	BACK
℄	BASELINE
BM	BENCHMARK
BRG	BEARING
℄	CENTERLINE
CS	CURVE TO SPIRAL
e	SUPERELEVATION RATE (CROSS SLOPE)
EQ	EQUALITY
EXT	EXTERNAL
HCL	HORIZONTAL CONTROL LINE
HSD	HEADLIGHT SIGHT DISTANCE
L	LENGTH OF CIRCULAR CURVE
LS	LENGTH OF SPIRAL
LVC	LENGTH OF VERTICAL CURVE
E	CENTER CORRECTION OF VERTICAL CURVE
ML	MAIN LINE
MO	MIDDLE ORDINATE
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
POL	POINT ON LINE
PSD	PASSING SIGHT DISTANCE
PT	POINT OF TANGENT
PVC	POINT OF VERTICAL CURVE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENT
R	RADIUS
SC	SPIRAL TO CURVE
SSD	STOPPING SIGHT DISTANCE
ST	SPIRAL TO TANGENT
STA	STATION
T	TANGENT LENGTH
TGL	THEORETICAL GRADE LINE
TS	TANGENT TO SPIRAL
VC	VERTICAL CURVE

UTILITIES	
ABBR.	DESCRIPTION
E	ELECTRIC
EMH	ELECTRIC MANHOLE
FO	FIBER OPTIC
G	GAS
GP	GUY POLE
GSB	GAS SERVICE BOX (HOUSE LINE)
GV	GAS VALVE (MAIN LINE)
HYD	HYDRANT
LP	LIGHT POLE
LPC	LOW PRESSURE GAS
PP	POWER POLE
SA	SANITARY SEWER
SMH	SANITARY MANHOLE
ST	STORM SEWER
T	TELEPHONE
TCB	TRAFFIC CONTROL BOX
TELBOX	TELEPHONE BOX
TEL P	TELEPHONE POLE
TMH	TELEPHONE MANHOLE
CTV	CABLE TELEVISION
W	WATER
WSB	WATER SERVICE BOX (HOUSE LINE)
WV	WATER VALVE (MAIN LINE)

TOPOGRAPHY (MISCELLANEOUS)	
ABBR.	DESCRIPTION
AOBE	AS ORDERED BY ENGINEER
ASPH	ASPHALT
BDY	BOUNDARY
BLDG	BUILDING
BM	BENCH MARK
CC	CENTER TO CENTER
CONC	CONCRETE
CONST	CONSTRUCTION
CR	COUNTY ROAD
D	DEED DISTANCE
DM	DIRECT MEASUREMENT
DWY	DRIVEWAY
EP	EDGE OF PAVEMENT
ES	EDGE OF SHOULDER
FP	FENCE POST
FD	FOUNDATION
FL	FENCE LINE
FW	FEDERAL WETLAND
GAR	GARAGE
GR	GRAVEL
HB	HIGHWAY BOUNDARY
HO	HOUSE
HWY	HIGHWAY
IP	IRON PIN OR IRON PIPE
MB	MAILBOX
MON	MONUMENT
N&W	NAIL AND WASHER
OG	ORIGINAL GROUND
O/H	OVERHEAD
P	PARCEL
PVMT	PAVEMENT
PED POLE	PEDESTRIAN POLE
℄	PROPERTY LINE
POR	PORCH
RTE	ROUTE
ROW	RIGHT OF WAY
RW	RETAINING WALL
SH	STATE HIGHWAY
SHLDR	SHOULDER
SPK	SPIKE
ST	STREET
STK	STAKE
STY	STORY
S/W	SIDEWALK
TE	TEMPORARY EASEMENT
TO	TEMPORARY OCCUPANCY
U/G	UNDERGROUND
WW	WING WALL

TOPOGRAPHY (DRAINAGE)	
ABBR.	DESCRIPTION
BB	BOTTOM OF BANK (STREAM)
BC	BOTTOM OF CURB
BO	BOTTOM OF OPENING
CAP	CORRUGATED ALUMINUM PIPE
CB	CATCH BASIN
CIP	CAST IRON PIPE
CMP	CORRUGATED METAL PIPE
CP	CONCRETE PIPE
CSP	CORRUGATED STEEL PIPE
CULV	CULVERT
DIA	DIAMETER
DMH	DRAINAGE MANHOLE
DS	DRAINAGE STRUCTURE PIPE
D'XING	DITCH CROSSING
EL	ELEVATION
ELEV	ELEVATION
ES	END SECTION
HW	HEADWALL
INV	INVERT
MH	MANHOLE
RCP	REINFORCED CONCRETE PIPE
SICPP	SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE
TC	TOP OF CURB
TG	TOP OF GRATE
VCP	VITRIFIED CLAY PIPE

STANDARD SYMBOL (PLANS)	ITEM UNIT:	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
'	ft	LINEAR FEET
ftq	SF	SQUARE FEET
YDr	CY	CUBIC YARD
mi	MI	MILES
AC	AC	ACRES
lb	LB	POUND
TON	TON	TON
GAL	GAL	GALLON
EA	EA	EACH
SY	SY	SQUARE YARDS
LF	LF	LINEAR FEET

PROPOSED	
ABBR.	DESCRIPTION
CRM	CRUMB RUBBER MODIFIED
LDNG	LANDING
PG	PERFORMANCE GRADED
POB	POINT OF BEGINNING
POE	POINT OF ENDING
PPA	POLYPHOSPHORIC ACID
N	NORTHING
TYP	TYPICAL
EIC	ENGINEER IN CHARGE
CB	CATCH BASIN/DRAINAGE INLET
MH	MANHOLE
E	EASTING
LT	LEFT
RT	RIGHT

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C&S Engineers, Inc.  
21 Arkansas Street  
Plattsburgh, New York 12901  
Phone: 315-455-2000  
www.cscos.com



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MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S.GALLAGHER		
DESIGNED BY: K.MCARDELL		
CHECKED BY: T.HUMPHREY		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

**INDEX AND ABBREVIATIONS**

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**IAB-01**

SHEET 2 OF 59

FEATURE	SYMBOL
<b>ALIGNMENT</b>	
CONTROL (CENTERLINE)	
CENTER OF CURVATURE	
POINT OF CURVATURE	
POINT OF INTERSECTION	
POINT OF BEGINNING	
POINT OF CURVATURE	
POINT OF END	
POINT ON TANGENT	
POINT OF VERTICAL CURVATURE	
VERTICAL EVENT POINT	
VERTICAL HIGH POINT	
VERTICAL LOW POINT	
<b>CONTROL</b>	
BASELINE	
BASELINE, PROJECTION	
BASELINE, POINT	
BASELINE, POINT ON LINE	
BASELINE, SPUR POINT	
BASELINE, TIE POINT	
BENCHMARK	
POINT, SURVEY MARKER, PERM.	
<b>DRAINAGE</b>	
CULVERT PIPE	
CULVERT PIPE (DIR)	
INVERT	
STRUCTURE, RECTANGULAR	
STRUCTURE, INVERT	
STRUCTURE, MANHOLE	
STRUCTURE, ROUND	
<b>GEOTECHNICAL</b>	
DRILL HOLE	
<b>LANDSCAPE</b>	
AREA, BRUSH LINE	
AREA, HEDGE ROW	
AREA, PLANTING BED	
CUT LINE, TOP OF	
ELEVATION, SPOT	
FENCE	
FILL LINE, BOTTOM OF	
FLAG POLE	
MAILBOX	
POST, SINGLE	
SHRUB, CONIFEROUS	
SHRUB, DECIDUOUS	
TREE, CONIFEROUS	
TREE, DECIDUOUS	
TREE, STUMP	
WETLANDS	

FEATURE	SYMBOL
<b>ROW MAPPING</b>	
DEED LINE	
HIGHWAY BOUNDARY	
PROPERTY LOT LINE	
HIGHWAY BNDRY., APPROX.	
HIGHWAY BNDRY, PT.	
PT., FENCE LOCATION	
PT., IRON PIPE	
PT., IRON ROD	
PT., MONUMENT	
PT., NAIL	
PT., RAILROAD SPIKE	
PT., SPIKE	
PT., STAKE	
PT., TREE W/ WIRE	
PT., WALL LOCATION	
<b>SIGNS</b>	
SINGLE POST	
SINGLE POST, PROPOSED	
BACK TO BACK, PROPOSED	
<b>TRAFFIC CONTROL</b>	
SIGNAL, SPAN WIRE	
BOX, JUNCTION	
BOX, PULL BOX	
BOX, SPLICE	
MICROCOMPUTER CABINET	
PED POLE	
SIGNAL HEADS	
SIGNAL POLE	
<b>UTILITIES</b>	
ABANDONED UTILITY	
CONDUIT, UNDERGROUND	
ELECTRIC LINE, UNDERGROUND	
ELECTRIC LINE, OVERHEAD	
FIBER OPTIC, UNDERGROUND	
FIBER OPTIC, OVERHEAD	
GAS, UNDERGROUND	
SANITARY SEWER, UNDERGROUND	
COMBINED SEWER LINE	
STORM SEWER, UNDERGROUND	
STEAM	
STEAM RETURN	
CHLORINE LINE	
TELEPHONE, UNDERGROUND	
TELEPHONE, OVERHEAD	
CABLE TV, UNDERGROUND	
CABLE TV, OVERHEAD	
WATER LINE, UNDERGROUND	
UNKNOWN UTILITY	

FEATURE	SYMBOL
<b>UTILITIES</b>	
ELECTRIC, BOX	
ELECTRIC, METER	
ELECTRIC, MANHOLE	
ELECTRIC, POLE, TRANS.	
GAS, METER	
GAS, MANHOLE	
GAS, LINE MARKER	
GAS, VALVE	
GAS, VENT	
LIGHTING, POLE	
LIGHTING, POLE, PED.	
POLE, WITH UTILITY	
POLE, WITH LIGHT	
SANITARY SEWER MANHOLE	
TELEPHONE, LINE MARKER	
TELEPHONE, MANHOLE	
CABLE TV, LINE MARKER	
CABLE TV, PULL BOX	
WATER, FIRE HYDRANT	
WATER, VALVE	
UNKNOWN MANHOLE	
<b>ROADWAY</b>	
GUIDE RAIL, MISCELLANEOUS	
PARKING BUMPER	
ELEVATION, SPOT	



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**LEGEND**

**LEG-01**

SHEET 3 OF 59

## DEFINITIONS

- THE WORDS "SHALL", "SHOULD", AND "MAY", AS USED IN THE CONTRACT DOCUMENTS, HAVE THE FOLLOWING MEANINGS:
  - SHALL – A MANDATORY CONDITION. IN THE DESIGN, APPLICATION, OR LOCATION OF DEVICES REQUIREMENTS HAVING "SHALL" STIPULATIONS ARE MANDATORY. NO DISCRETION IN FOLLOWING THEM IS ALLOWED.
  - SHOULD – AN ADVISORY CONDITION. WHERE "SHOULD" IS USED IN RELATION TO A PROVISION, THAT PROVISION IS RECOMMENDED, AND NORMALLY IS TO BE FOLLOWED, BUT IS NOT MANDATORY. DEVIATION FROM SUCH PROVISIONS IS PERMISSIBLE IF, AND TO THE EXTENT, THERE IS JUSTIFIABLE CAUSE TO DO SO.
  - MAY – A PERMISSIBLE CONDITION. NO REQUIREMENT FOR DESIGN OR APPLICATION IS INTENDED.

## GENERAL NOTES

- CONSTRUCTION AND MATERIALS SPECIFICATIONS: STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS, NEW YORK STATE DEPARTMENT OF TRANSPORTATION, OFFICE OF ENGINEERING, ADOPTED JANUARY 1, 2021.
- THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE CITY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE CITY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR. ANY SALVAGED MATERIAL TO REMAIN THE PROPERTY OF THE CITY SHALL BE PICKED UP BY CITY FORCES AT NO EXPENSE TO THE CONTRACTOR.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM DIMENSIONS IN THE FIELD PRIOR TO ANY NEW CONSTRUCTION OR REPLACEMENT.
- ALL DISTURBED AREAS ADJACENT TO CURBING, DRIVEWAY AND SIDEWALK INSTALLATION SHALL BE RESTORED BY THE TREATMENT SHOWN ON THE PLANS, BY TOPSOIL AND SODDING, OR AS ORDERED BY ENGINEER.
- WORK LIMITS ARE SHOWN ON THE GENERAL PLANS. THE ACTUAL LIMITS OF WORK SHALL BE CONFIRMED IN THE FIELD AT THE TIME OF CONSTRUCTION. THE ENGINEER SHALL DEFINE SPECIFIC LIMITS WHERE QUESTIONS ARISE.
- SAWCUTTING SHALL BE REQUIRED FOR ALL AT ABUTTING SIDEWALK EDGES TO BE RETAINED, AT PAVEMENT TERMINATIONS, AND ALONG CURBS TO BE REPLACED OR RESET. ALL SAWCUTS IN STREETS ARE TO BE SEALED WITH ASPHALT FILLERS PER NYS DOT 705-0500 OR 702-3401.
- THE ROADWAY SHALL BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES. ROADWAY CLEANING SHALL BE PERFORMED AOB.
- MATERIALS, EQUIPMENT AND VEHICLES SHALL NOT BE STORED OR PARKED WITHIN 10 FEET OF THE CURBLINE BEFORE WORK BEGINS OR AFTER THE CONTRACTOR'S OPERATIONS ARE SHUT DOWN. NO CONSTRUCTION MATERIALS MAY BE STORED OR PLACED ON THE ROADWAY OR ROADBED EXCEPT WITHIN A PROTECTED WORK AREA.

## NYS DOT PAYMENT ITEMS

- ALL ITEM NUMBERS INCLUDED IN THE CONTRACT DOCUMENTS ARE FOR SPECIFICATION REFERENCE. WHEN QUANTITIES ARE LISTED THEY ARE FOR REFERENCE TO THE ENGINEER'S ESTIMATE.
- THIS PROJECT IS TO BE BID AS A LUMP SUM. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF ALL QUANTITIES USED TO ESTABLISH A BID PRICE FOR THE PROJECT.

## UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES ENCOUNTERED IN THIS WORK, WHERE NECESSARY THE CONTRACTOR SHALL PROVIDE TIMBER, PLANK OR OTHER APPROVED MATERIALS AND SECURELY BRACE AND PROTECT THESE UTILITIES.

## UTILITY ADJUSTMENTS

- ALL MANHOLE COVERS AND DRAINAGE STRUCTURE GRATES SHALL BE ADJUSTED TO FINAL GRADE AND SET FLUSH PER ITEM 604.070701. ALL WATER VALVE COVERS SHALL BE ADJUSTED TO FINAL GRADE UNDER ITEM 663.33.
- THE REMAINING UTILITY MANHOLES, VALVES, ETC WILL BE ADJUSTED BY THE RESPECTIVE UTILITY OWNERS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY THESE FEATURES AND COORDINATE THE ADJUSTMENT WITH THE AFFECTED UTILITY OWNERS. SEE COORDINATION WITH UTILITIES NOTE IN THE SPECIAL NOTES.
- THE CONTRACTOR SHALL EXERCISE CARE WHEN EXCAVATING AROUND UTILITY FRAMES, GRATES, AND COVERS. SHOULD THESE ITEMS BECOME DAMAGED DUE TO THE CONTRACTOR'S OPERATIONS, THEY SHALL BE REPAIRED OR REPLACED AT NO COST TO THE UTILITY OWNER OR THE CITY.

## PERMITS

- A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED FOR THIS PROJECT. THE CONTRACTOR IS REQUIRED TO PROVIDE AND MAINTAIN SOIL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH SECTION 209 OF THE NYS DOT STANDARD SPECIFICATIONS.

## DRAINAGE FACILITIES

- THE CONTRACTOR SHALL BECOME FAMILIAR WITH THE DRAINAGE CHARACTERISTICS OF EACH STREET SEGMENT INCLUDED IN THE PROJECT SO THAT WORK MAY PROGRESS EFFICIENTLY WITH FULL KNOWLEDGE OF THE POTENTIAL DRAINAGE PROBLEMS, ALL DRAINAGE FACILITIES, INCLUDING CULVERTS, GUTTERS, DRAINAGE PIPES AND DRAINAGE STRUCTURES, WITHIN THE CONTRACT LIMITS SHALL BE KEPT CLEAN AND FREE FLOWING FOR THE DURATION OF THE CONTRACT, AOB.
- THE CONTRACTOR SHALL FIRST CLEAN ALL DRAINAGE STRUCTURES WHERE INDICATED IN THE DRAINAGE TABLES AND THEN CONFIRM THE REPAIR OR REPLACEMENT DIMENSIONS AND QUANTITIES NEEDED FOR EACH STRUCTURE AND PIPE INVERT ELEVATIONS FOR REPLACEMENT STRUCTURES.

## EMERGENCY REPAIRS

- THE CONTRACTOR SHALL PROVIDE THE LOCAL POLICE THE NAME, ADDRESS AND TELEPHONE NUMBER OF A PERSON OR PERSONS AUTHORIZED OUTSIDE THE NORMAL CONTRACT WORKING HOURS TO SECURE AND USE LABOR, MATERIAL AND EQUIPMENT FOR EMERGENCY REPAIRS TO MAKE SAFE THE ENTIRE AREA OF THE CONTRACT. DUPLICATE COPIES OF SUCH NOTICES SHALL BE FILED WITH THE EIC, AND THE CITY OF PLATTSBURGH ENGINEERING TECHNICIAN, ANDREW DURRIN (518) 536-7453.

## HOT MIX ASPHALT

80 SERIES COMPACTION:

- THE CONTRACTOR SHOULD BE AWARE THAT THIS IS A PERFORMANCE RELATED SPECIFICATION IN WHICH THE CONTRACTOR IS RESPONSIBLE FOR COMPACTING THE PAVEMENT TO WITHIN A SPECIFIED DENSITY RANGE. IN ORDER TO SUCCESSFULLY COMPACT THE PAVEMENT TO THE SPECIFIED DENSITY RANGE, THE CONTRACTOR MUST BE PREPARED TO SELECT, OPERATE, AND CONTROL THE PAVING AND COMPACTION EQUIPMENT, TO MONITOR THE RESULTS, AND TO MAKE NECESSARY ADJUSTMENTS (WITHOUT DIRECTION FROM THE ENGINEER) TO ACHIEVE THE DESIRED RESULTS.

## PG BINDER AND MIX DESIGN LEVEL

REQUIREMENTS OF THIS NOTE APPLY TO ALL SECTION 402 HOT MIX ASPHALT (HMA) ITEMS IN THIS CONTRACT.

- PG BINDER – USE A PG 64S-22 (STANDARD) MEETING THE REQUIREMENTS OF AASHTO M 332, STANDARD SPECIFICATIONS FOR PERFORMANCE GRADED ASPHALT BINDER USING MULTIPLE STRESS CREEP RECOVERY (MSCR), FOR THE PRODUCTION OF HOT MIX ASPHALT MIXTURES FOR THIS PROJECT. TERMINAL BLEND CRUMB RUBBER MODIFIER MAY BE USED FOR THIS PG BINDER.

WHEN TERMINAL BLEND CRM PG BINDER IS USED, THE FOLLOWING SHALL APPLY:

- CRUMB RUBBER PARTICLES SHALL BE FINER THAN #30 SIEVE SIZE.
- THE CRM PG BINDER SHALL BE STORAGE-STABLE AND HOMOGENEOUS.
- THE DYNAMIC SHEAR RHEOMETER (DSR) SHALL BE SET AT 2-MM GAP.
- THE CRM PG BINDER SHALL BE 99% FREE OF PARTICLES RETAINED ON THE 600 μM SIEVE AS TESTED IN ACCORDANCE WITH SECTION 5.4 OF M 332.

NOTE: THE PG BINDER FOR THIS PROJECT MAY BE MODIFIED WITH CRM ADDITIVES TO MEET THE REQUIREMENTS STATED ABOVE. HANDLING OF THE HMA SHALL BE DISCUSSED AT PRE-CONSTRUCTION AND PRE-PAVING MEETINGS.

- PPA PROHIBITION – USE OF POLYPHOSPHORIC ACID (PPA) TO MODIFY THE PG BINDER PROPERTIES IS PROHIBITED FOR MIXTURES UNDER THIS CONTRACT. THIS PROHIBITION ALSO APPLIES TO THE USE OF PPA AS A CROSS-LINKING AGENT FOR POLYMER MODIFICATION.
- MIX DESIGN – THE MIXTURE DESIGNS MUST BE DEVELOPED IN ACCORDANCE WITH THE CRITERIA SPECIFIED IN THE HMA ITEMS THAT ARE APPROPRIATE FOR THE MIXTURE DESIGN LEVEL OF 75 GYRATIONS.

## CONCRETE SIDEWALKS, DRIVEWAYS AND CURB RAMPS

- SIDEWALK CURB RAMPS INSTALLED UNDER THIS PROJECT SHALL CONFORM TO THE DETAILS ON NYS DOT STANDARD SHEET 608-01 (12 SHEETS). THE CONTRACTOR SHALL INSTALL SURFACE APPLIED DETECTABLE WARNING UNITS ITEM 608.20 AT ALL CURB RAMP LOCATIONS SHOWN ON THE PAVEMENT PLANS AND THE TABLE OF CONCRETE SIDEWALK ON DWG MST-01.
- CONCRETE SIDEWALKS AND DRIVEWAYS ARE TO BE GENERALLY PLACED IN LENGTHS THAT HAVE CONTROL JOINTS THAT CREATE 5 FOOT BY 5 FOOT BLOCKS OR 4 FOOT BY 4 FOOT DEPENDING ON LOCATION. AT STREET INTERSECTIONS THE SIDEWALK IS IRREGULARLY SHAPED TO CONFORM TO THE CORNER RADII. AT THESE AND ANY OTHER SIMILAR LOCATIONS, AND WHERE UTILITY COVERS PENETRATE THE SIDEWALK, ADDITIONAL CONTROL JOINTS SHALL BE PLACED TO CONTROL CRACKING. ADDITIONAL CONTROL JOINT LOCATIONS ARE TO BE DETERMINED BY THE CONTRACTOR. ANY NEW SIDEWALK THAT CRACKS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- ALL EXPANSION JOINTS SHALL HAVE PREMOULDED RESILIENT JOINT FILLER MATERIAL PLACED BELOW THE SURFACE AND THEN SEALED WITH AN APPROVED CONCRETE SEALER MEETING NYS DOT MATERIAL SPECIFICATION SECTION 705-06.

## DISPOSAL OF CONSTRUCTION AND DEMOLITION DEBRIS

- THERE ARE NO AREAS WITHIN THE CONTRACT LIMITS AVAILABLE FOR DISPOSAL OF DEBRIS.

## STAGING AREAS

- THERE IS NO DESIGNATED ON-SITE AREA FOR THE CONTRACTOR TO UTILIZE FOR STAGING OF EQUIPMENT AND OPERATIONS.

## HIGHWAY WORK PERMITS

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED STATE, COUNTY, AND MUNICIPAL PERMITS WITHIN HIGHWAY RIGHTS-OF-WAY. DRIVEWAY RELEASES FROM INDIVIDUAL PROPERTY OWNERS WILL BE SECURED BY THE CITY PRIOR TO BIDDING.

## SURVEY AND STAKEOUT

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SURVEYING AND STAKEOUT TO ACCURATELY LOCATE ALL PROPOSED IMPROVEMENTS PER THE REQUIREMENTS OF ITEM 625.01, SURVEY OPERATIONS.



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GENERAL  
NOTES

GEN-01

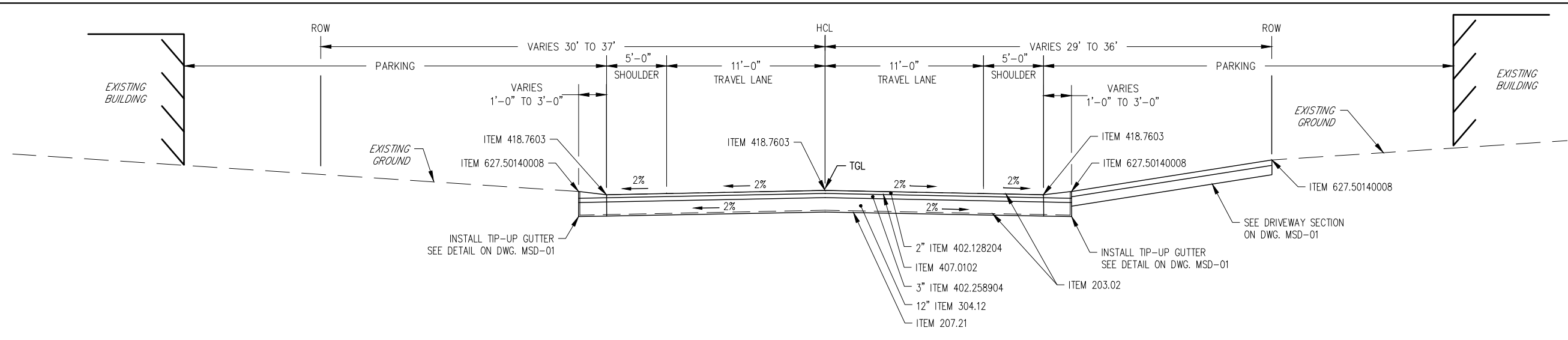
SHEET 4 OF 59



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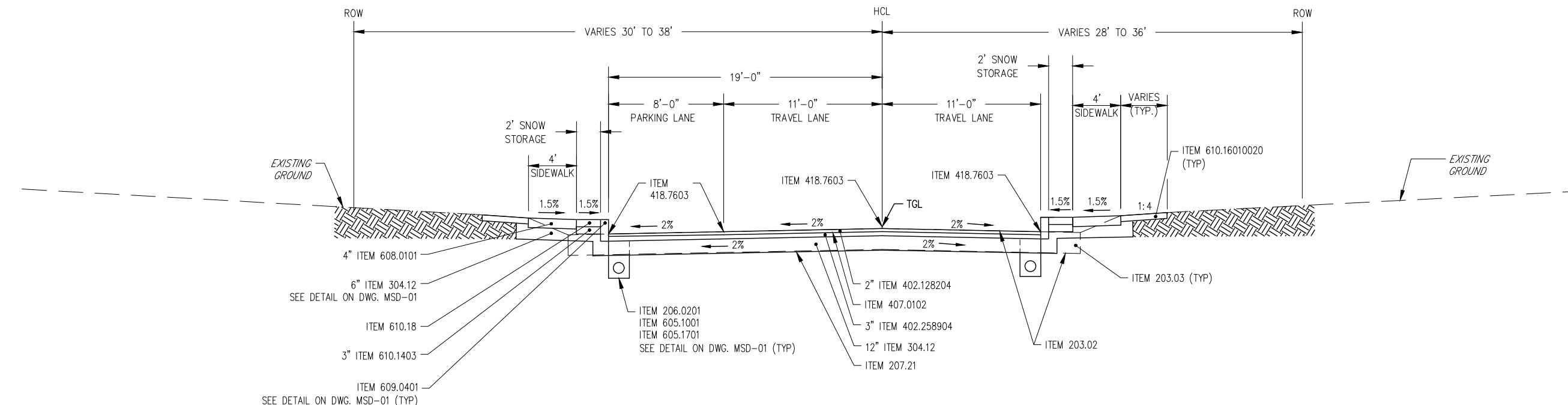


**COGAN AVENUE  
RECONSTRUCTION**  
CITY OF PLATTSBURGH



**TYPICAL NORMAL CROWN RECONSTRUCTION SECTION**

STA. 0+99 TO STA. 5+25



**TYPICAL NORMAL CROWN RECONSTRUCTION SECTION WITH PARKING LANE**

STA. 5+25 TO STA. 18+11

ITEM NO.	DESCRIPTION	UNIT	NOTES
203.02	UNCLASSIFIED EXCAVATION & DISPOSAL	CY	1. TACK COAT SHALL BE APPLIED BETWEEN ALL LIFTS OF SUPERPAVE HMA, AND VERTICAL FACES OF CURB BELOW FINISHED GRADE, AT A RATE OF 0.03 - 0.04 GAL/SY.
203.03	EMBANKMENT IN PLACE	CY	
206.0201	TRENCH AND CULVERT EXCAVATION	CY	
207.21	GEOTEXTILE SEPARATION	SY	
304.12	SUBBASE COURSE, TYPE 2	CY	
402.128204	12.5 F2 TOP COURSE HMA, 80 SERIES COMPACTION	TON	
402.258904	25 F9 BINDER COURSE HMA, 80 SERIES COMPACTION	TON	2. PROFILE AND CROSS SLOPE SHALL BE CONTROLLED BY A TAUT REFERENCE LINE OR A.O.B.E.
407.0102	DILUTED TACK COAT	GAL	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	3. THE EXCAVATION OF UNSUITABLE PAVEMENT AND SHOULDER AREAS NOT SHOWN ON THE PLANS MAY BE NECESSARY, AS DETERMINED BY THE ENGINEER.
605.1001	UNDERDRAIN FILTER TYPE 2	LF	
605.1701	OPTIONAL UNDERDRAIN PIPE, 4 INCH DIAMETER	CY	4. SAWCUT SHALL BE PERPENDICULAR TO PAVEMENT SURFACE AND STRAIGHT. ALL SAWCUT PAVEMENT SURFACES SHALL BE TACK COATED PRIOR TO FINAL PAVING.
608.0101	CONCRETE SIDEWALKS AND DRIVEWAYS	LF	
609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF150	CY	5. TRANSITION CROSS SLOPE FROM EXISTING CORNELIA STREET PROFILE AT STA. 0+99 TO NORMAL CROWN AT STA. 1+21. TRANSITION CROSS SLOPE FROM NORMAL CROWN AT STA. 17+64 TO EXISTING CROWN AT STA. 18+11.
610.1403	TOPSOIL - LAWNS	LF	
610.16010020	TURF ESTABLISHMENT - PERFORMANCE	SY	6. USE SAME PAVEMENT SECTION AS ABOVE FOR FULL DEPTH RECONSTRUCTION OF ALL SIDE STREETS TO LIMITS OF WORK.
610.18	SODDING	LF	
627.50140008	CUTTING PAVEMENT	LF	

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MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: K. MCARDELL		
DESIGNED BY: K. MCARDELL		
CHECKED BY: T. HUMPHREY		

NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW

**TYPICAL  
SECTIONS**

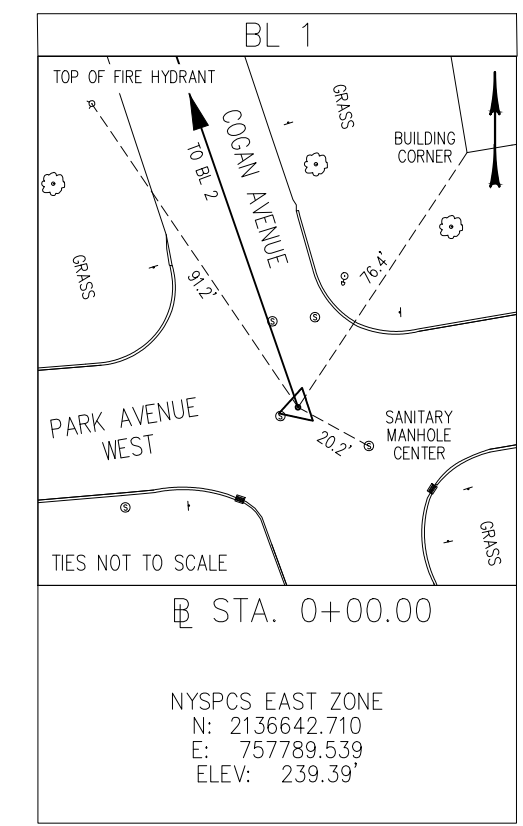
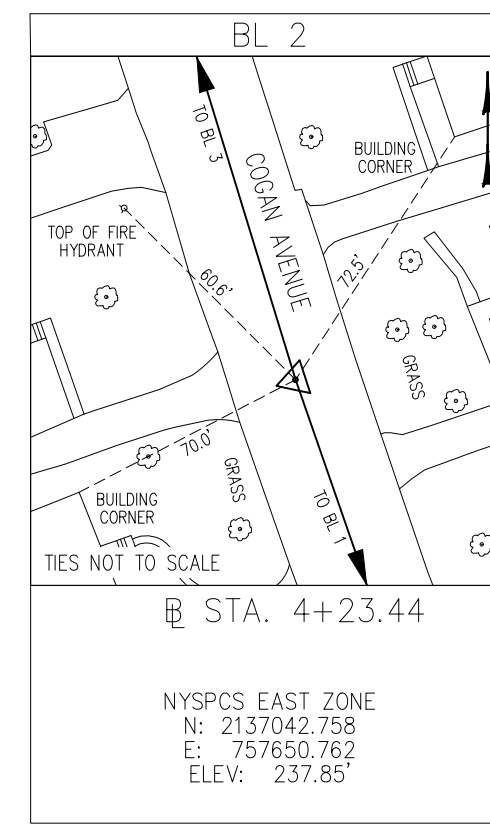
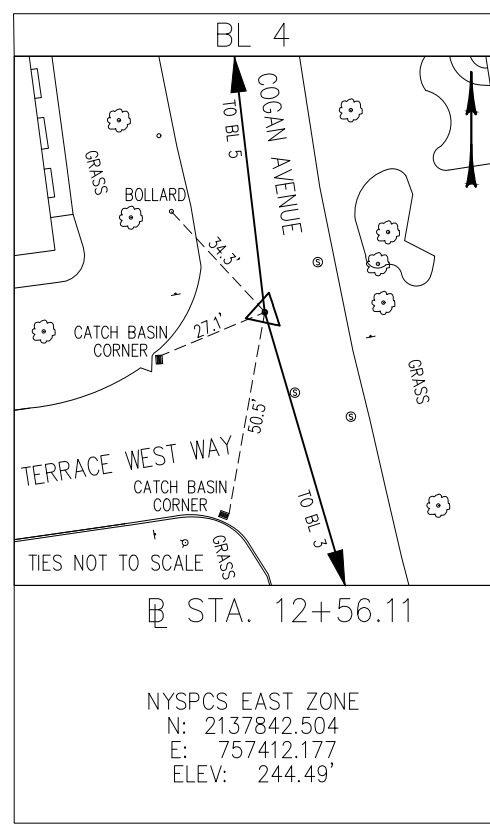
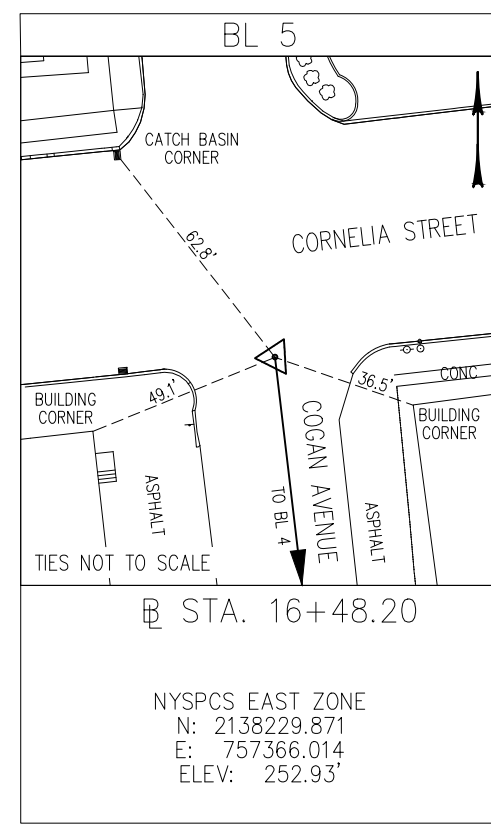
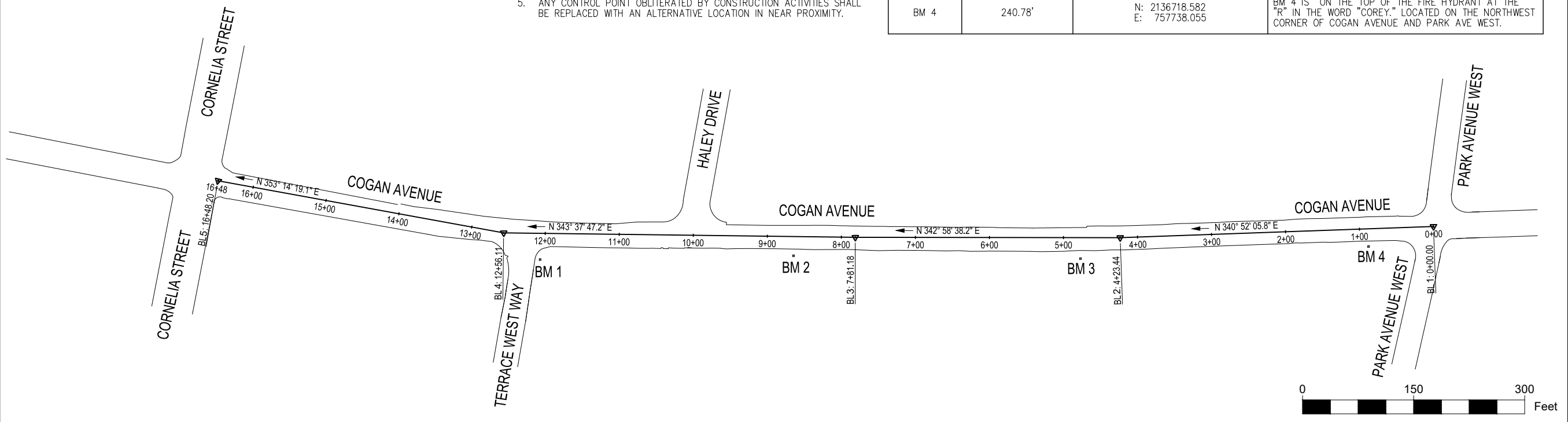
**TYP-01**

SHEET 5 OF 59



- NOTES:
1. THE HORIZONTAL DATUM IS REFERRED TO THE NORTH AMERICAN DATUM OF 1983 (2011), NEW YORK STATE PLANE COORDINATE SYSTEM, EASTERN ZONE (3101).
  2. VERTICAL DATUM IS NAVD88.
  3. POINTS ARE SUBJECT TO FROST HEAVE. ALL ELEVATIONS SHOULD BE FIELD VERIFIED PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES.
  4. PRIOR TO BEGINNING ANY WORK, THE CONTRACTOR IS RESPONSIBLE FOR THE VERIFICATION OF SURVEY CONTROL USED IN THE VERTICAL AND HORIZONTAL POSITIONING OF DESIGNING IMPROVEMENTS AND FOR IMMEDIATELY NOTIFYING THE E.I.C. OF ANY DISCREPANCIES FOUND.
  5. ANY CONTROL POINT OBLITERATED BY CONSTRUCTION ACTIVITIES SHALL BE REPLACED WITH AN ALTERNATIVE LOCATION IN NEAR PROXIMITY.

BENCHMARK TABLE			
NO.	ELEV.	COORDINATES	DESCRIPTION
BM 1	247.36'	N: 2137782.895 E: 757392.330	BM 1 IS ON THE TOP OF THE FIRE HYDRANT AT THE "P" IN THE WORD "OPEN." LOCATED ON THE SOUTHWEST CORNER OF COGAN AVENUE AND TERRACE WEST WAY
BM 2	243.66'	N: 2137457.234 E: 757497.785	BM 2 IS ON THE TOP OF THE FIRE HYDRANT AT THE "R" IN THE WORD "COREY." LOCATED ON THE WEST SIDE OF COGAN AVENUE.
BM 3	241.20'	N: 2137085.618 E: 757607.988	BM 3 IS ON THE TOP OF THE FIRE HYDRANT AT THE "R" IN THE WORD "COREY." LOCATED ON THE WEST SIDE OF COGAN AVENUE.
BM 4	240.78'	N: 2136718.582 E: 757738.055	BM 4 IS ON THE TOP OF THE FIRE HYDRANT AT THE "R" IN THE WORD "COREY." LOCATED ON THE NORTHWEST CORNER OF COGAN AVENUE AND PARK AVE WEST.



C&S Engineers, Inc.  
 21 Arkansas Street  
 Plattsburgh, New York 12901  
 Phone: 315-455-2000  
 www.cscos.com



**COGAN AVENUE  
 RECONSTRUCTION**  
 CITY OF PLATTSBURGH

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PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S.GALLAGHER		
DESIGNED BY: K.MCARDELL		
CHECKED BY: T.HUMPHREY		
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**BASELINE TIES  
 AND BENCHMARKS**

**BLT-01**

SHEET 6 OF 59

**GENERAL NOTES**

1. THE CONTRACTOR SHALL MAINTAIN TRAFFIC THROUGHOUT THE LENGTH OF THE CONTRACT IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 OF THE CURRENT NYS DOT STANDARD SPECIFICATIONS, THE NATIONAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) WITH THE NEW YORK STATE (NYS) SUPPLEMENT, THE TEMPORARY TRAFFIC CONTROL DETAILS IN THE PLANS AND PROPOSAL OF THIS CONTRACT AND AS ORDERED BY THE RESIDENT ENGINEER (AOBE).
2. FOR TYPICAL APPLICATIONS OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION AREAS NOT SPECIFIED IN THE PLANS, THE PROVISIONS OF PART 6 OF THE MUTCD WITH NYS SUPPLEMENT SHALL APPLY. THE STANDARDS OF APPLICATION NOTED THEREIN AND ON THE PLANS ARE TO BE CONSIDERED MINIMUM STANDARDS.
3. PRIOR TO THE START OF ANY WORK OPERATIONS, ALL RELATED WORK FOR PROPOSED TEMPORARY TRAFFIC CONTROL SHALL BE COMPLETE. THIS INCLUDES BUT IS NOT LIMITED TO, ALL SIGNS, SIGNALS, PAVEMENT MARKINGS, BARRIERS, DELINEATION (CONES, DRUMS, ETC.), FLAGGERS, PAVEMENT MODIFICATIONS AND ANY OTHER RELATED WORK AS DIRECTED BY THE RESIDENT ENGINEER.
4. WEEDS, SHRUBBERY, CONSTRUCTION MATERIALS, EQUIPMENT, VEHICLES OR WORKERS SHALL NOT OBSCURE TRAFFIC CONTROL DEVICES OR OBSTRUCT TRAFFIC. THE CONTRACTOR SHALL BE REQUIRED TO TRIM ANY FOLIAGE OBSTRUCTING THE VISIBILITY OF ANY TRAFFIC CONTROL DEVICES WHETHER PERMANENT, TEMPORARY OR CONSTRUCTION.
5. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TEMPORARY TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE ENGINEER OR DESIGNEE FIVE (5) WORKING DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE BASIC CONCEPT OF THE TRAFFIC CONTROL PLAN. SUCH CONCEPTUAL CHANGES MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL BY THE CITY OF PLATTSBURGH DPW SUPERINTENDENT OR DESIGNEE.

**ACTIVITY AREA AND WORK AREA COORDINATION**

1. THE CONTRACTOR SHALL COORDINATE ANY WORK WITH OTHER CONTRACTORS, UTILITY COMPANIES, OR MUNICIPALITIES IN ORDER TO ENSURE ALL TRAFFIC IS PROPERLY AND SAFELY MAINTAINED AND PROTECTED AT ALL TIMES.

**CONTRACTOR VEHICLES**

1. VEHICLES OR EQUIPMENT SHALL NOT TO BE PARKED ALONG THE ROADWAY USED BY THE GENERAL PUBLIC, OR ANY OTHER AREAS DEEMED HAZARDOUS BY THE RESIDENT ENGINEER.
2. VEHICLES BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED IN A MANNER WHICH OBSTRUCTS SIGNS, BARRIERS, BARRICADES, OTHER TRAFFIC CONTROL DEVICES.

**SIGNS**

1. ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE NATIONAL MUTCD WITH NYS SUPPLEMENT. THE LOCATIONS OF THE SIGNS SHOWN ON THE TEMPORARY TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE RESIDENT ENGINEER.
2. ANY EXISTING SIGNS WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AOBE. BLANK COVERS USED TO COVER PORTIONS OF EXISTING SIGNS SHALL BE OF A COLOR AND REFLECTORIZED MATERIAL MATCHING THAT OF THE SIGN BEING PARTIALLY COVERED. THE CONTRACTOR SHALL BE LIABLE FOR ANY DAMAGE TO SIGNS CAUSED BY THE METHODS USED TO TEMPORARILY REMOVE, RELOCATE OR COVER SIGN PANELS OR SIGN TEXT, AT NO ADDITIONAL COST TO THE CITY. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. ALL CONSTRUCTION SIGNS SHALL BE MOUNTED ON NCHRP350 APPROVED TEMPORARY SIGN SUPPORTS UNLESS SHIELDED BY TEMPORARY CONCRETE BARRIER OR GUIDE RAIL, AND LOCATED BEYOND THE DEFLECTION DISTANCES. SIGN SUPPORTS FABRICATED PER STANDARD SHEET 619-08 SHALL BE CONSIDERED NCHRP 350 APPROVED.
5. IF CONSTRUCTION RELATED TRAFFIC CONGESTION BACKS UP BEYOND THE INSTALLED ADVANCE SIGN SEQUENCE, ADDITIONAL ADVANCE SIGNING SHALL BE PLACED IN ADVANCE OF THE CONGESTION.
6. UNDER NO CIRCUMSTANCES SHALL A SIGN PANEL BE TRUNCATED OR TRIMMED SO THAT IT WILL NOT INTERFERE WITH TRAVEL WAY. IF THIS CONDITION EXISTS, THE RESIDENT ENGINEER WILL DIRECT THE CONTRACTOR TO RELOCATE THAT SIGN AS NOT TO CAUSE ANY OBSTRUCTION WITH THE TRAVEL WAY.
7. WARNING FLAGS ON SIGNS MAY BE USED TO INCREASE TARGET VALUE AND VISIBILITY OF WORK ZONE SIGNS DURING DAYLIGHT HOURS.
8. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED ON STANDARD SHEET 619-12. THESE DIMENSIONS MAY BE INCREASED WHENEVER NECESSARY FOR GREATER LEGIBILITY OR EMPHASIS.

**DELINEATION AND GUIDING DEVICES**

1. WHERE POSSIBLE ALL DELINEATION AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 1 FT LATERAL CLEARANCE (2 FT DESIRABLE) TO THE TRAVELED WAY.
2. DEFORMED OR DAMAGED CHANNELIZING DEVICES THAT DO NOT MAINTAIN APPEARANCE, COLOR AND REFLECTIVITY WILL BE EVALUATED FOR ACCEPTABILITY IN ACCORDANCE WITH THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) "QUALITY GUIDELINES FOR WORK ZONE TRAFFIC CONTROL DEVICES". AS THE CONDITION OF DEVICES DETERIORATE OVER TIME, THOSE THAT FALL BELOW THE MARGINAL CATEGORY SHALL BE REPLACED.

**PUBLIC ACCESS**

1. ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES, UNLESS OTHER AGREEMENTS, SUITABLE TO THE OWNERS, CAN BE MADE. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVE. THE CONTRACTOR SHALL MAINTAIN ACCESS TO COMMERCIAL DRIVEWAYS AT ALL TIMES WHEN A FACILITY IS IN USE. FOR MULTIPLE ACCESS PROPERTIES, ONLY ONE DRIVEWAY MAY BE CLOSED AT ONE TIME. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA. THE MATERIAL SHALL CONSIST OF CRUSHED STONE OR ASPHALT CONCRETE, WHICHEVER IS DEEMED MORE APPROPRIATE FOR THE CIRCUMSTANCE, AOBE.

**LANE CLOSURES**

1. THERE IS SUFFICIENT WIDTH ON COGAN AVENUE TO MAINTAIN ALTERNATING ONE-WAY TRAFFIC UNDER FLAGGING OPERATION WITH LANE CLOSURES FOR WORK AREAS, USING THE APPROPRIATE SETUP FROM SHEET WZTC-03. MAINTAIN 10 FOOT MINIMUM LANE WIDTHS.
2. IF, IN THE OPINION OF THE RESIDENT ENGINEER, SHORT TERM LANE CLOSURES ARE CREATING EXCESSIVE DELAYS TO TRAFFIC OR ARE CREATING A SAFETY CONCERN DUE TO SLOW OR STOPPED TRAFFIC, AFFECTED LANE CLOSURES SHALL BE REMOVED AND THE ROADWAY OPENED TO TRAFFIC.
3. ROADSIDE WORK NOT REQUIRING A FULL LANE CLOSURE CAN BE PROTECTED USING THE LANE NARROWING DETAIL ON DWG. WZTC-05.

**BICYCLISTS AND PEDESTRIANS**

1. THE NEEDS AND CONTROL OF ALL ROAD USERS (MOTORISTS, BICYCLISTS, AND PEDESTRIANS WITHIN THE HIGHWAY INCLUDING PERSONS WITH DISABILITIES IN ACCORDANCE WITH THE AMERICAN WITH DISABILITIES ACT OF 1990 (ADA) TITLE II, PARAGRAPH 35.130) THROUGH A WORK ZONE SHALL BE AN ESSENTIAL PART OF THE HIGHWAY CONSTRUCTION, UTILITY WORK, MAINTENANCE, AND THE MANAGEMENT OF TRAFFIC INCIDENTS.
2. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN A USABLE TRAVEL PATH FOR PEDESTRIANS AND BICYCLISTS EITHER THROUGH OR AROUND WORK ZONES FOR THE DURATION OF THIS PROJECT, IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. PEDESTRIAN ACCESS TO RESIDENTIAL AND COMMERCIAL PROPERTIES SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION.

**NIGHT TIME CONSTRUCTION**

1. NO NIGHT WORK WILL BE ALLOWED UNLESS PRIOR APPROVAL IS GIVEN BY THE CITY. ADDITIONAL WZTC WILL BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.

**BUMPS**

1. A W8-1 "BUMP" SIGN SHALL BE PLACED ON ALL APPROACHES WHERE A BUMP IS PRESENT IN THE ROADWAY. WHERE NUMEROUS BUMPS OR DIPS OCCUR, THE W8-1 "BUMP" SIGN SHALL BE REPLACED WITH A W8-8 "ROUGH ROAD" SIGN.

**EMERGENCY REPAIRS**

1. RESPONSIBILITY FOR EMERGENCY REPAIRS-THE CONTRACTOR SHALL SUBMIT TO THE APPROPRIATE LAW ENFORCEMENT AND EMERGENCY AGENCIES IN WRITING THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE PERSON OR PERSONS AUTHORIZED OUTSIDE THE NORMAL CONTRACT WORKING HOURS TO SECURE MATERIAL AND EQUIPMENT FOR EMERGENCY REPAIRS TO MAKE SAFE THE ENTIRE CONTRACT AREA. DUPLICATE COPIES OF THE ABOVE SHALL BE FILED WITH THE RESIDENT ENGINEER, AND THE CITY OF PLATTSBURGH.

**TEMPORARY DRAINAGE**

1. IN ACCORDANCE WITH ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL, THE CONTRACTOR SHALL MAINTAIN DRAINAGE FACILITIES AND OTHER HIGHWAY ELEMENTS, OLD OR NEW. THE CONTRACTOR SHALL DEVOTE PARTICULAR ATTENTION TO ALL DRAINAGE FACILITIES KEEPING THEM FULLY OPERATIONAL AT ALL TIMES. ACCEPTABLE CONTROLS MUST BE IN PLACE TO ADEQUATELY DRAIN THE ROADWAY AND THE REMAINDER OF RIGHT-OF-WAY AREAS.

**SIGNALIZED INSECTIONS**

1. WHEN CONSTRUCTION OPERATIONS AFFECT SIGNALIZED INTERSECTIONS, THE TRAFFIC SIGNAL SHALL BE TURNED OFF BEFORE PROCEEDING WITH THE FLAGGING OPERATION, AND ANY EXISTING W3-3 SIGNS COVERED. FLAGGING SHALL BE PER DETAIL ON DWG. WZTC-04.
2. THE CONTRACTOR MUST RETURN THE SIGNAL TO A 3-COLOR OPERATION AT THE END OF THE WORK OPERATION. IF THE SIGNAL DOES NOT RETURN TO 3-COLOR OPERATION, THE CONTRACTOR SHALL NOTIFY THE CITY OF PLATTSBURGH PMLD AND DPW AND MUST CONTINUE FLAGGING UNTIL THE SIGNAL CREW ARRIVES AND THE SIGNAL IS REPAIRED.
3. REFER TO THE TRAFFIC SIGNAL PLANS FOR ADDITIONAL NOTES.

**FLAGGERS**

1. WHEN FLAGGERS ARE BEING USED FOR TRAFFIC CONTROL PURPOSES, FLAGGER SIGNS (W20-7) SHALL BE PLACED AS SHOWN ON PROPOSAL OR AOBE ON ALL APPROACHES TO THE AREAS BEING FLAGGED. FLAGGER SIGNS SHALL NOT BE USED FOR BRIEF PERIODS OF INCIDENTAL FLAGGING. FLAGGER SIGNS SHALL NOT BE VISIBLE WHEN FLAGGERS ARE NOT BEING USED.
2. STOP/SLOW PADDLES SHALL BE USED FOR FLAGGING AND SHALL CONFORM TO THE REQUIREMENTS IN THE MUTCD BUT SHALL BE A MINIMUM OF 24 INCHES.
3. FLAGGERS SHALL NOT POSITION THEMSELVES DIRECTLY IN THE PATH OF ONCOMING TRAFFIC. THE FLAGGER SHOULD STAND EITHER ON THE SHOULDER ADJACENT TO THE TRAFFIC BEING CONTROLLED OR IN THE CLOSED/CHANNELIZED LANE. AT ALL TIMES, THE CONTRACTOR SHALL PROVIDE FLAGGERS WITH A CLEAR ESCAPE PATH, FREE OF PARKED VEHICLES, EQUIPMENT OR OTHER OBSTACLES.
4. THE CONTRACTOR SHALL PROVIDE FLAGGERS WHERE SIGHT DISTANCES ARE IMPAIRED BY THE OPERATION OR IN ANY OTHER SITUATION AOBE. FLAGGERS SHALL BE REQUIRED TO USE TWO-WAY RADIOS, WALKIE-TALKIES, OR OTHER FORMS OF ENHANCED COMMUNICATION WHEN ONE FLAGGER IS NOT VISIBLE TO THE OTHER OR IF THE ENGINEER DEEMS IT NECESSARY.
5. FLAGGERS SHALL USE THE FREE HAND FOR EMPHASIS AS SHOWN IN PART 6 OF THE MUTCD. EACH FLAGGER SHALL CONTROL NO MORE THAN ONE APPROACH OF TRAFFIC.
6. THE RESIDENT ENGINEER SHALL DETERMINE THE NUMBER OF FLAGGERS NEEDED FOR EACH WORK ZONE. UNDER CERTAIN GEOMETRIC AND TRAFFIC SITUATIONS, MORE THAN ONE FLAGGER STATION MAY BE REQUIRED FOR EACH DIRECTION OF TRAFFIC.
7. FLAGGER STATIONS SHOULD BE VISIBLE FAR ENOUGH AHEAD TO PERMIT ALL VEHICLES TO STOP. THE FLAGGER SHOULD BE STATIONED FAR ENOUGH AHEAD OF THE WORK TO WARN THEM WITH AIR HORNS, TO COMPLY WITH CURRENT SPECIFICATIONS OF APPROACHING DANGER, SUCH AS VEHICLES OUT OF CONTROL.
8. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES AND FLAGGERS SHALL BE PROVIDED.
9. WHEN A PEDESTRIAN APPROACHES A FLAGGER STATION, THE FLAGGER SHALL STOP TRAFFIC AND DIRECT THE PEDESTRIAN TO A SAFE ROUTE THROUGH THE WORK AREA. FLAGGERS SHALL COORDINATE THE FLAGGING OF THE WORK ZONE TO ENSURE PEDESTRIANS CAN SAFELY PROCEED THROUGH THE AREA.

**OPEN CUTS IN PAVEMENT**

1. ALL ROADWAY AREAS TEMPORARILY CLOSED FOR FULL DEPTH REPAIRS, TRENCHING FOR CURB WORK, DRIVEWAY EXCAVATION, EXCAVATION FOR WATER LINE PLACEMENT OR EXCAVATION REQUIRED FOR DRAINAGE STRUCTURE INSTALLATIONS SHALL BE REOPENED AT THE END OF THE WORK DAY. AT ALL TIMES WHEN WORK IS NOT IN PROGRESS, ALL OPENINGS SHALL BE BACKFILLED WITH TYPE A STONE IN ACCORDANCE WITH SECTION 203-3.15 OR TO A DEPTH AS ORDERED BY THE ENGINEER.
2. TEMPORARY HOT MIX ASPHALT WEDGES SHALL BE REQUIRED AT PAVEMENT TERMINATION AREAS IN ORDER TO PROVIDE A SMOOTH TRAVELED WAY. THE TAPER RATE BETWEEN THE EXISTING AND MILLED SURFACES SHALL BE 1 ON 30. THE WEDGE MATERIAL SHALL BE THE SAME AS THE TOP COURSE ITEM USED IN THE CONTRACT OR EQUIVALENT, AOBE. WORK SHALL INCLUDE NECESSARY CLEANING, TACK COAT, HMA PLACEMENT AND REMOVAL.



C&S Engineers, Inc.  
21 Arkansas Street  
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**COGAN AVENUE RECONSTRUCTION**  
**CITY OF PLATTSBURGH**

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DESIGNED BY:		J. DEORDIO
CHECKED BY:		T. HUMPHREY
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**WORK ZONE TRAFFIC CONTROL NOTES**

**WZTC-01**

SHEET 7 OF 59

**PORTABLE VARIABLE MESSAGE SIGNS**

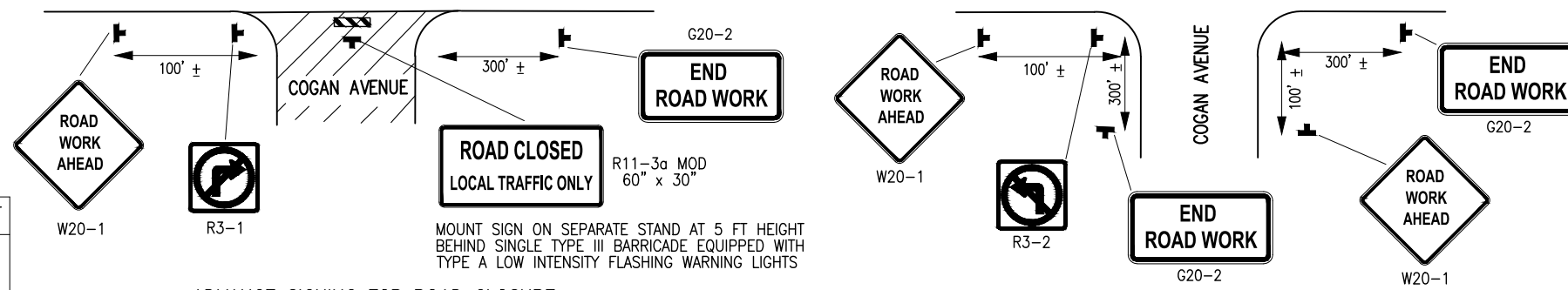
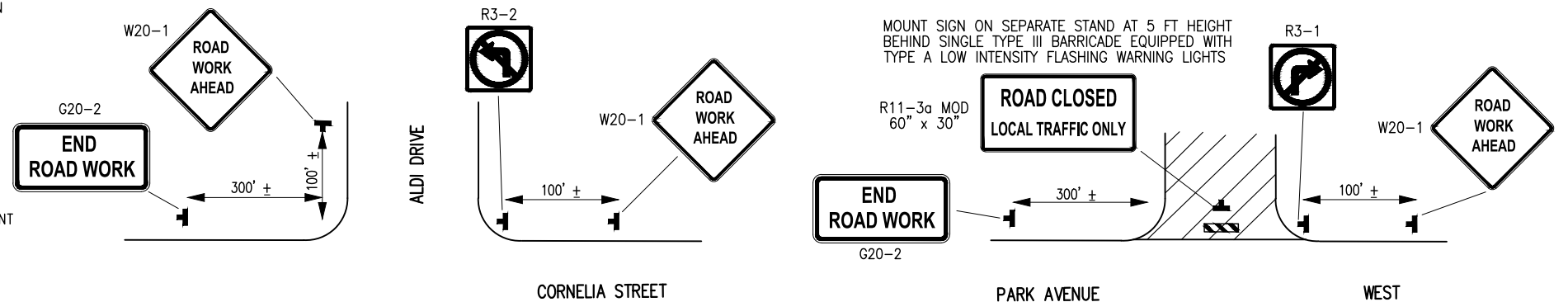
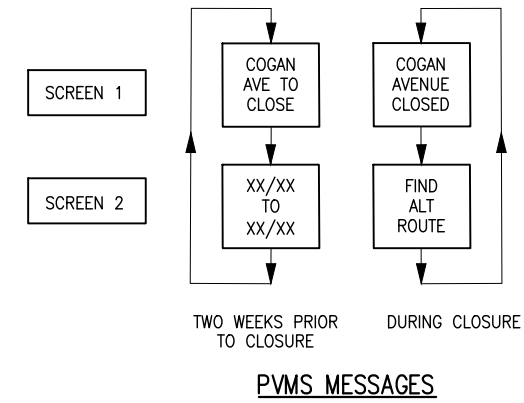
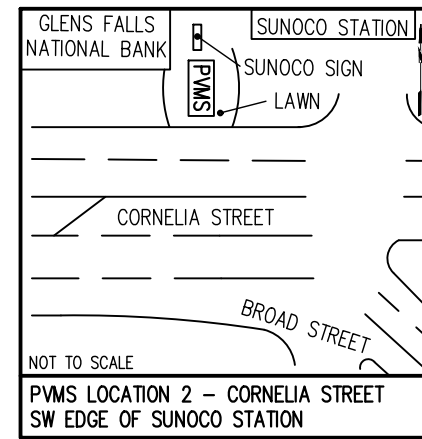
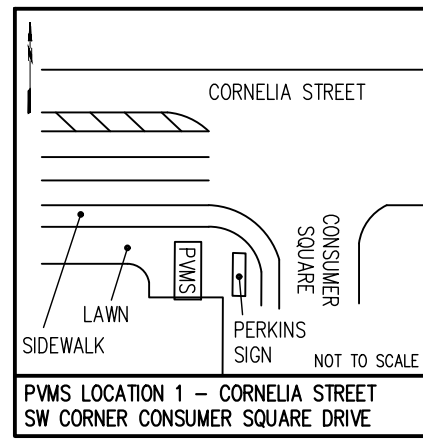
1. THE CONTRACTOR SHALL PROVIDE TWO PVMS UNITS FOR USE DURING THIS CONTRACT.
2. ITEM C619.111121 AMENDS THE NYS DOT STANDARD SPECIFICATION FOR ITEM 619.111121 IN THIS WAY:  
 "UPON COMPLETION OF THE CONTRACT, THE PVMS UNIT(S) AND ASSOCIATED HARDWARE SHALL BECOME THE PROPERTY OF THE CITY OF PLATTSBURGH. CONTRACTOR SHALL DELIVER UNIT(S) TO THE CITY DPW YARD AT 208 IDAHO AVENUE."
3. THE USE OF PORTABLE VARIABLE MESSAGE SIGNS (PVMS) IS INTENDED TO ADVISE TRAFFIC OF UPCOMING AND ACTUAL CHANGES TO REGULAR TRAFFIC PATTERNS.
4. THE PVMS SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE WORK ZONE TRAFFIC CONTROL (WZTC) PLANS AND OTHER LOCATIONS SPECIFIED BY THE ENGINEER. THE PVMS SHALL BE PLACED FOURTEEN (14) DAYS BEFORE ANY ROAD CLOSURE AND WILL REMAIN IN PLACE UNTIL ALL WORK WITHIN A DESIGNATED WZTC PHASE HAS BEEN COMPLETED. FOURTEEN (14) DAYS PRIOR TO A CLOSURE TAKING PLACE, THE INITIAL MESSAGE SHOWN IN THE CONTRACT PLANS SHALL BE DISPLAYED. ONCE THE CLOSURE HAS TAKEN PLACE, THE PVMS'S SHALL REMAIN ON FOR THE DURATION OF THE CLOSURE DISPLAYED WITH THE MESSAGE SHOWN IN THE PLANS.
5. PVMS UNITS TO BE LOCATED WITHIN SHOULDER CLOSURE PER STANDARD SHEET 619-22 WHEN LOCATED WITHIN CLEAR ZONE.
6. PVMS UNITS LOCATED BEHIND GUIDE RAIL SHALL NOT BE LOCATED WITHIN RAIL DEFLECTION DISTANCE. SEE SECTION 619 OF THE STANDARD SPECIFICATIONS.
7. PVMS UNITS SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH SECTION 619 OF THE STANDARD SPECIFICATION AND ITEM C619.111121.
8. SEE DETAILS ON THIS SHEET FOR PORTABLE VARIABLE MESSAGE SIGN (PVMS) LOCATIONS AND MESSAGES.

**ROAD CLOSURE AND DETOUR**

1. THE CONTRACTOR SHALL CLOSE COGAN AVENUE TO THROUGH TRAFFIC FOR MAJOR WORK INCLUDING: WATER LINE REPLACEMENT, DRAINAGE INSTALLATION, AND FULL DEPTH PAVEMENT RECONSTRUCTION.
2. THERE IS NO SIGNED DETOUR.
3. THE CONTRACTOR MAY USE LANE NARROWING AND LANE CLOSURES DEPICTED ON DWGS. WZTC-03, WZTC-04, AND WZTC-05 FOR THE FOLLOWING OPERATIONS: TOP COURSE PAVING, STRIPING, SIGN INSTALLATION, DRIVEWAY PAVING, TURF ESTABLISHMENT, TREE REMOVALS AND PLANTINGS, SIDEWALK CONSTRUCTION, AND SIGNAL IMPROVEMENTS.
4. POST ADVANCE SIGNING PER DETAILS ON THIS SHEET.
5. CONTRACTOR SHALL INSTALL AND OPERATE PVMS UNITS TWO WEEKS IN ADVANCE OF CLOSING COGAN AVENUE.

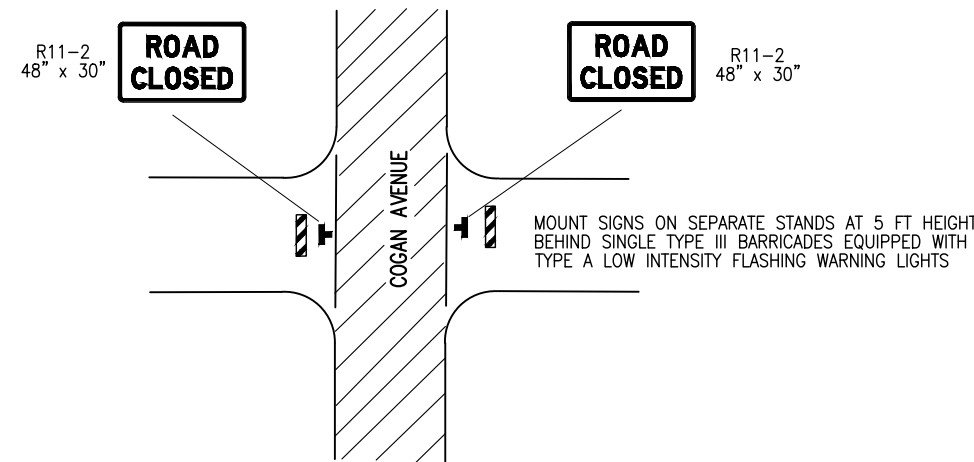
**WORK ZONE TRAFFIC CONTROL ITEMS**

ITEM NO	DESCRIPTION	UNIT
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS
619.04	TYPE III CONSTRUCTION BARRICADE	EA
C619.111121	PORTABLE VARIABLE MESSAGE SIGN (PVMS) STANDARD SIZE - LINE OR CHARACTER MATRIX (LED), RADAR, NO CELLULAR COMMUNICATIONS REQUIRED (CITY SPECIFICATIONS)	EA



**ADVANCE SIGNING FOR ROAD CLOSURE NORTH PROJECT LIMITS**

**ADVANCE SIGNING FOR ROAD CLOSURE SOUTH PROJECT LIMITS**



**ADVANCE SIGNING FOR ROAD CLOSURE INTERSECTING STREETS**



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 Phone: 315-455-2000  
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**COGAN AVENUE RECONSTRUCTION**  
 CITY OF PLATTSBURGH

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S. GALLAGHER		
DESIGNED BY: J. DEORDIO		
CHECKED BY: T. HUMPHREY		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

**WORK ZONE TRAFFIC CONTROL NOTES**

**WZTC-02**

SHEET 8 OF 59

Mer 04, 2021 - 8:53am  
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Plattsburgh, New York 12901  
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COGAN AVENUE RECONSTRUCTION  
CITY OF PLATTSBURGH

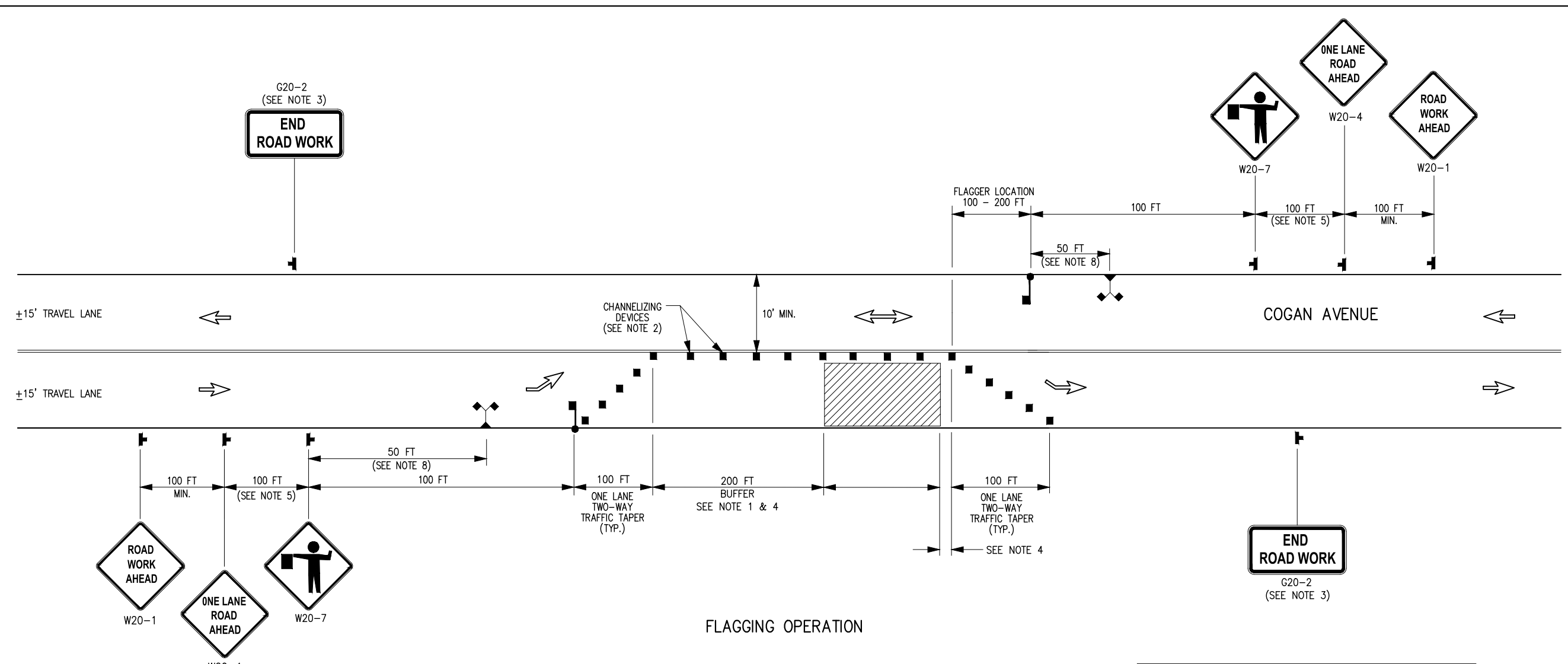
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MARK	DATE	DESCRIPTION
REVISIONS		
		PROJECT NO: A54.003.001
		DATE: MARCH 2021
		DRAWN BY: S. GALLAGHER
		DESIGNED BY: J. DEORDIO
		CHECKED BY: T. HUMPHREY
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**WORK ZONE TRAFFIC CONTROL PLAN**

**WZTC-03**

SHEET 9 OF 59



**FLAGGING OPERATION**

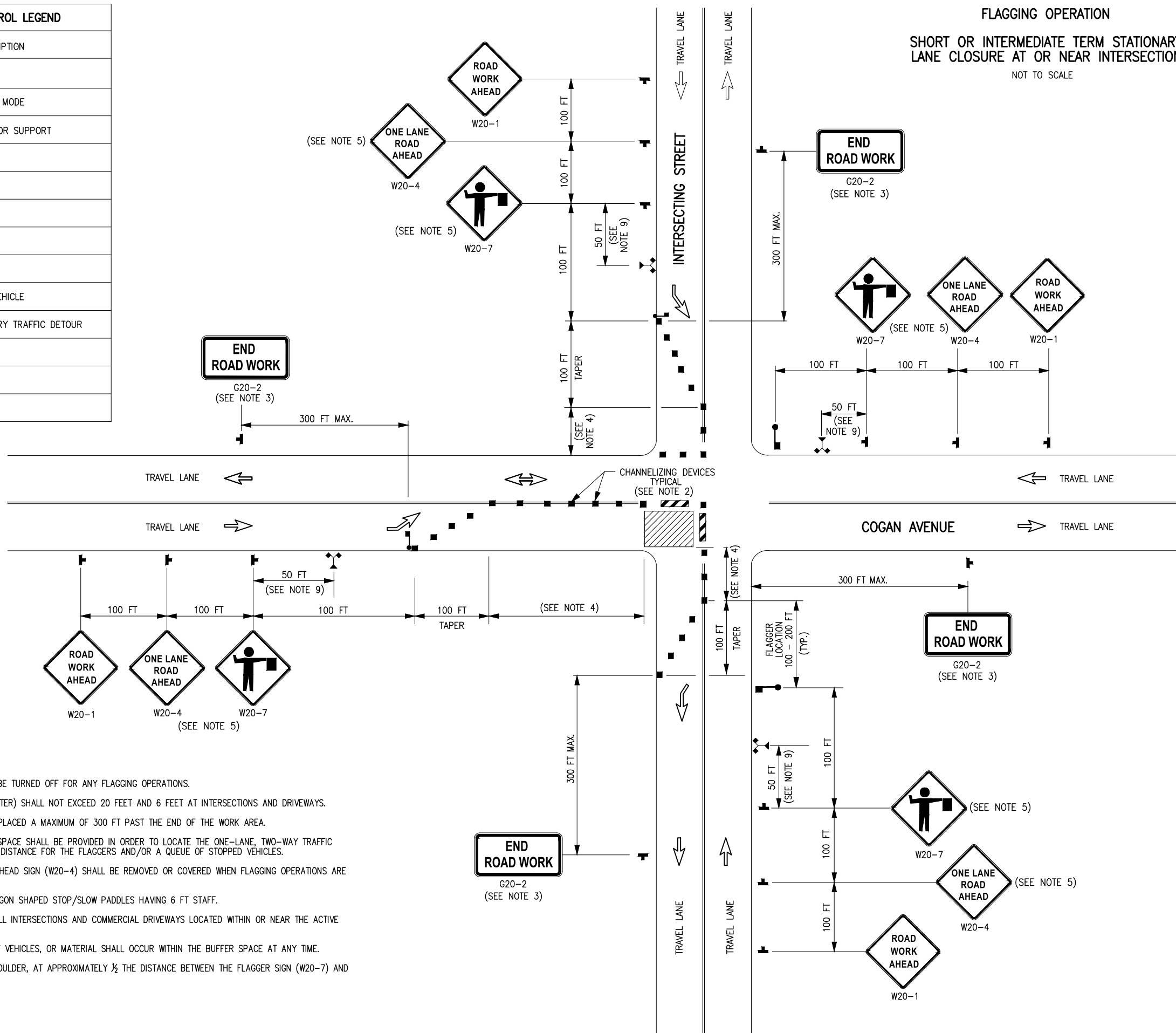
SHORT OR INTERMEDIATE TERM STATIONARY LANE CLOSURE ON 2-LANE 2-WAY ROADWAY  
NOT TO SCALE

**NOTES:**

1. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
2. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20 FEET AND 6 FEET AT INTERSECTIONS AND DRIVEWAYS.
3. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 300 FT PAST THE END OF THE WORK SPACE.
4. WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
5. FLAGGER SIGN (W20-7) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED OR COVERED WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
6. ALL FLAGGERS SHALL USE 24 INCH (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6 FT STAFF.
7. WHEN A SIDE ROAD INTERSECTS THE MAINLINE ROADWAY BETWEEN THE W20-1 AND W20-7 SIGNS, EACH SIDE ROAD APPROACH SHALL BE SIGNED WITH ROAD WORK AHEAD (W20-1) AT A 100 FT DISTANCE UPSTREAM FROM THE INTERSECTION AND END ROAD WORK (G20-2). WHERE THE SIDE ROAD INTERSECTS THE MAINLINE ROADWAY BETWEEN THE W20-7 FLAGGER SIGNS, EACH SIDE ROAD APPROACH SHALL BE SIGNED WITH A FLAGGER (W20-7) SIGN AT A 100 FT DISTANCE UPSTREAM FROM THE INTERSECTION, ROAD WORK AHEAD (W20-1) AT A 100 FT DISTANCE UPSTREAM FROM THE FLAGGER SIGN, END ROAD WORK (G20-2) SIGNS, AND A FLAGGER POSTED. END ROAD WORK SIGNS SHALL BE WITHIN 500 FT DOWNSTREAM FROM THE INTERSECTION, ADDITIONAL FLAGGERS SHALL BE POSTED AT ALL COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE LANE CLOSURE.
8. THE FLAG TREE SHALL BE LOCATED IN THE SHOULDER, APPROXIMATELY 1/2 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7) AND THE FLAGGER.
9. THIS DETAIL IS APPLICABLE FOR WORK ON COGAN AVENUE AND SHOULD BE USED IN COMBINATION WITH THE SETUP ON SHEET WZTC-04 FOR WORK NEAR INTERSECTIONS AS APPROPRIATE.
10. FLAGGERS SHALL ALLOW PEDESTRIANS THROUGH THE WORK AREA CLOSURE BY STOPPING TRAFFIC IN BOTH DIRECTIONS.

WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANNELIZING DEVICE
	SIGN, TEMPORARY
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK SPACE
	SHADOW OR BARRIER VEHICLE
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	FLAGGER
	FLAG TREE

WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANNELIZING DEVICE
	SIGN, TEMPORARY
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK SPACE
	SHADOW OR BARRIER VEHICLE
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	FLAGGER
	FLAG TREE



NOTES:

1. AT SIGNALIZED INTERSECTIONS, SIGNALS SHALL BE TURNED OFF FOR ANY FLAGGING OPERATIONS.
2. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20 FEET AND 6 FEET AT INTERSECTIONS AND DRIVEWAYS.
3. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 300 FT PAST THE END OF THE WORK AREA.
4. WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
5. FLAGGER SIGN (W20-7) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED OR COVERED WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
6. ALL FLAGGERS SHALL USE 24 INCH (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6 FT STAFF.
7. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK AREA.
8. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
9. THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROXIMATELY 1/2 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7) AND THE FLAGGER.



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CITY OF PLATTSBURGH

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**WORK ZONE TRAFFIC CONTROL PLAN**

**WZTC-04**

SHEET 10 OF 59

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**COGAN AVENUE  
 RECONSTRUCTION**  
 CITY OF PLATTSBURGH

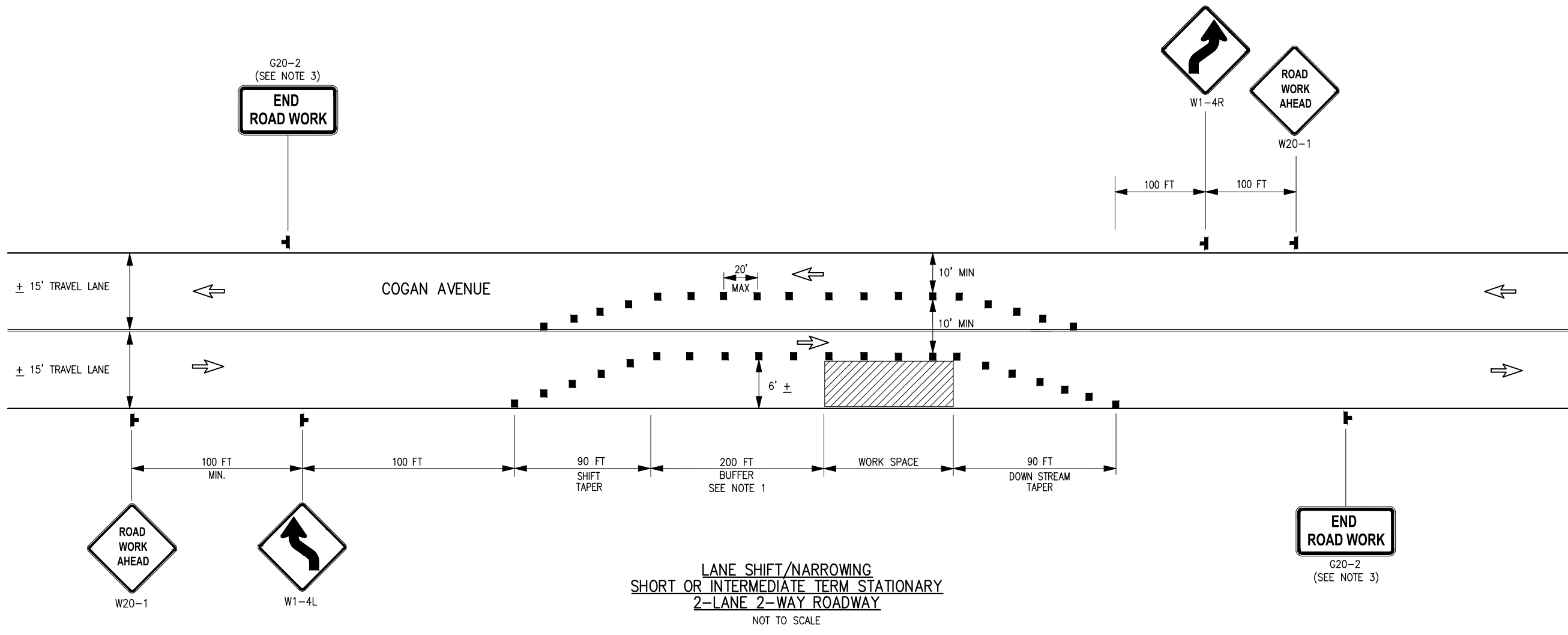
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**WORK ZONE  
 TRAFFIC CONTROL  
 PLAN**

**WZTC-05**

SHEET 11 OF 59



**LANE SHIFT/NARROWING  
 SHORT OR INTERMEDIATE TERM STATIONARY  
 2-LANE 2-WAY ROADWAY**  
 NOT TO SCALE


**NOTES:**

- NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
- CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20 FEET AND 6 FEET AT INTERSECTIONS AND DRIVEWAYS.
- THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 300 FEET PAST THE END OF THE WORK SPACE.
- WHEN THE MINIMUM LANE WIDTH OF 10 FEET CANNOT BE MAINTAINED DUE TO A SHOULDER CLOSURE, USE THE DETAIL FOR SHORT TERM OR INTERMEDIATE TERM STATIONARY FLAGGING OPERATION SHOWN ON SHEET WZTC-03.
- CONSTRUCTION PERSONNEL SHALL ESCORT PEDESTRIANS THROUGH THE WORK AREA LANE NARROWING.


WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANNELIZING DEVICE
	SIGN, TEMPORARY
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK SPACE
	SHADOW OR BARRIER VEHICLE
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	FLAGGER
	FLAG TREE

TABLE OF CURBING								
ITEM 609.00401- CAST-IN-PLACE CONCRETE CURB TYPE VF 150								
	FROM STATION	OFFSET (FT)	TO STATION	OFFSET (FT)	RADIUS (FT)	CENTER OF RADIUS	LENGTH (FT)	
GNP-01	0+43.0	28.3 RT	0+43.0	40.3 RT			12	
	0+43.0	40.3 RT	0+43.0	42.3 RT			2	
	0+43.0	42.3 RT	0+43.4	52.3 RT			10	
	0+43.4	52.3 RT	0+43.4	54.3 RT			2	
	0+43.4	54.3 RT	0+43.3	58.3 RT			4	
GNP-02	0+39.7	30.8 RT	1+19.7	11.0 RT	20	1+06.7	32	
	1+00.0	31.0 LT	1+20.2	11.0 LT	20	1+06.0	32	
GNP-03	3+40.5	19.0 LT	3+40.5	24.0 LT			5	
	3+67.5	19.0 LT	3+67.5	24.0 LT			5	
	5+44.2	15.0 LT	5+44.2	10.0 LT			4	
	5+48.2	16.0 LT	5+50.2	16.0 LT			2	
	5+50.2	16.0 LT	5+62.4	16.0 LT			12	
	5+62.4	16.0 LT	5+75.5	19.0 LT			13	
	5+75.5	19.0 LT	6+40.5	19.0 LT			65	
	6+40.5	19.0 LT	6+42.5	19.0 LT			2	
	6+42.5	19.0 LT	6+50.0	19.0 LT			7.5	
	11+17.4	16.8 LT	10+32.9	14.9 LT			85	
	5+36.9	34.1 RT	5+62.8	11.0 RT	25	5+44.6	35	
	5+62.8	11.0 RT	5+78.3	11.0 RT			15	
GNP-04	5+78.3	11.0 RT	5+84.3	20.8 RT			2	
	5+84.3	17.6 RT	5+84.3	20.8 RT			3	
	5+80.3	11.0 RT	6+20.0	11.0 RT			40	
	6+16.0	17.6 RT	6+16.0	20.8 RT			3	
	6+20.0	11.0 RT	6+22.0	11.0 RT			2	
	6+22.0	11.0 RT	6+50.0	11.0 RT			28	
	6+50.0	19.0 LT	6+69.5	19.0 LT			19.5	
	6+69.5	19.0 LT	6+71.5	19.0 LT			2	
	6+71.5	19.0 LT	7+04.3	19.0 LT			33	
	7+04.3	19.0 LT	7+32.0	39.8 LT	25	7+23.5	35	
	7+32.0	39.8 LT	7+35.1	57.8 LT			18	
	7+35.1	57.8 LT	7+70.0	48.2 LT			4	
GNP-05	7+70.0	48.2 LT	7+94.5	19.0 LT	25	7+76.0	44	
	7+94.5	19.0 LT	9+00.0	19.0 LT			105.5	
	9+00.0	11.0 RT	9+164.0	11.0 RT			14	
	9+164.0	11.0 RT	9+166.0	11.0 RT			2	
	9+166.0	11.0 RT	9+166.0	11.0 RT			26	
	9+166.0	11.0 RT	9+194.0	11.0 RT			2	
	9+194.0	11.0 RT	7+54.0	11.0 RT			60	
	7+54.0	11.0 RT	7+56.0	11.0 RT			2	
	7+56.0	11.0 RT	7+80.0	11.0 RT			24	
	7+80.0	11.0 RT	7+82.0	11.0 RT			2	
	7+82.0	11.0 RT	7+92.0	11.0 RT			10	
	7+92.0	11.0 RT	7+94.0	11.0 RT			2	
GNP-06	7+94.0	11.0 RT	8+26.0	11.0 RT			32	
	8+26.0	11.0 RT	8+28.0	11.0 RT			2	
	8+28.0	11.0 RT	8+87.0	11.0 RT			59	
	8+87.0	11.0 RT	8+89.0	11.0 RT			2	
	8+89.0	11.0 RT	9+00.0	11.0 RT			11	
	9+00.0	19.0 LT	9+08.0	19.0 LT			8	
	9+08.0	19.0 LT	9+10.0	19.0 LT			2	
	9+10.0	19.0 LT	9+34.0	19.0 LT			24	
	9+34.0	19.0 LT	9+36.0	19.0 LT			2	
	9+36.0	19.0 LT	9+90.0	19.0 LT			54	
	9+90.0	19.0 LT	9+92.0	19.0 LT			2	
	9+92.0	19.0 LT	10+17.0	19.0 LT			25	
GNP-07	10+17.0	19.0 LT	10+19.0	19.0 LT			2	
	10+19.0	19.0 LT	10+26.5	19.0 LT			7.5	
	10+26.5	19.0 LT	10+26.5	19.0 LT			2	
	10+26.5	19.0 LT	10+58.0	19.0 LT			31.5	
	10+58.0	19.0 LT	10+58.0	19.0 LT			2	
	10+58.0	19.0 LT	11+50.0	19.0 LT			92	
	11+50.0	11.0 RT	9+16.0	11.0 RT			16	
	9+16.0	11.0 RT	9+18.0	11.0 RT			2	
	9+18.0	11.0 RT	10+21.0	11.0 RT			103	
	10+21.0	11.0 RT	10+23.0	11.0 RT			2	
	10+23.0	11.0 RT	10+54.0	11.0 RT			31	
	10+54.0	11.0 RT	10+56.0	11.0 RT			2	
GNP-08	10+56.0	11.0 RT	10+59.5	11.0 RT			3.5	
	10+59.5	11.0 RT	10+61.5	11.0 RT			2	
	10+61.5	11.0 RT	10+93.0	11.0 RT			31.5	
	10+93.0	11.0 RT	10+95.0	11.0 RT			2	
	10+95.0	11.0 RT	11+00	11.0 RT			55	
	SUB TOTAL							1436


TABLE OF CURBING (CONTINUED)							
ITEM 609.00401- CAST-IN-PLACE CONCRETE CURB TYPE VF 150							
	FROM STATION	OFFSET (FT)	TO STATION	OFFSET (FT)	RADIUS (FT)	CENTER OF RADIUS	LENGTH (FT)
GNP-09	11+50.0	19.0 LT	11+51.0	19.0 LT			1
	11+51.0	19.0 LT	11+53.0	19.0 LT			2
	11+53.0	19.0 LT	11+72.0	19.0 LT			19
	11+72.0	19.0 LT	11+74.0	19.0 LT			2
	11+74.0	19.0 LT	12+18.0	19.0 LT			44
	12+18.0	19.0 LT	12+20.0	19.0 LT			2
	12+20.0	19.0 LT	12+40.0	19.0 LT			20
	12+40.0	19.0 LT	12+42.0	19.0 LT			2
	12+42.0	19.0 LT	12+80.0	19.0 LT			38
	12+80.0	19.0 LT	12+82.0	19.0 LT			2
	12+82.0	19.0 LT	13+00.0	19.0 LT			18
	13+00.0	19.0 LT	13+02.0	19.0 LT			2
GNP-10	13+02.0	19.0 LT	13+41.0	19.0 LT			39
	13+41.0	19.0 LT	13+43.0	19.0 LT			2
	13+43.0	19.0 LT	13+61.0	19.0 LT			18
	13+61.0	19.0 LT	13+63.0	19.0 LT			2
	13+63.0	19.0 LT	13+66.0	19.0 LT			33
	13+66.0	19.0 LT	13+98.0	19.0 LT			32
	13+98.0	19.0 LT	14+00.0	19.0 LT			2
	14+00.0	11.0 RT	11+91.0	11.0 RT			41
	11+91.0	11.0 RT	11+93.0	11.0 RT			2
	11+93.0	11.0 RT	12+14.0	11.0 RT			21
	12+14.0	11.0 RT	12+16.0	11.0 RT			2
	GNP-11	12+16.0	11.0 RT	12+40.0	11.0 RT		
12+40.0		11.0 RT	12+42.0	11.0 RT			2
12+42.0		11.0 RT	12+72.0	11.0 RT			30
12+72.0		11.0 RT	12+74.0	11.0 RT			2
12+74.0		11.0 RT	13+10.0	11.0 RT			36
13+10.0		11.0 RT	13+12.0	11.0 RT			2
13+12.0		11.0 RT	13+34.0	11.0 RT			22
13+34.0		11.0 RT	13+36.0	11.0 RT			2
13+36.0		11.0 RT	13+68.0	11.0 RT			32
13+68.0		11.0 RT	13+70.0	11.0 RT			2
13+70.0		11.0 RT	13+80.0	11.0 RT			10
13+80.0		11.0 RT	13+82.0	11.0 RT			2
GNP-12	13+82.0	11.0 RT	13+91.0	11.0 RT			2
	13+91.0	11.0 RT	14+00.0	11.0 RT			9
	14+00.0	19.0 LT	14+18.0	19.0 LT			18
	14+18.0	19.0 LT	14+20.0	19.0 LT			2
	14+20.0	19.0 LT	14+60.0	19.0 LT			40
	14+60.0	19.0 LT	14+62.0	19.0 LT			2
	14+62.0	19.0 LT	14+81.0	19.0 LT			19
	14+81.0	19.0 LT	14+83.0	19.0 LT			2
	14+83.0	19.0 LT	15+20.0	19.0 LT			37
	15+20.0	19.0 LT	15+22.0	19.0 LT			2
	15+22.0	19.0 LT	15+40.0	19.0 LT			18
	15+40.0	19.0 LT	15+42.0	19.0 LT			2
GNP-13	15+42.0	19.0 LT	15+70.0	19.0 LT			28
	15+70.0	19.0 LT	15+72.0	19.0 LT			2
	15+72.0	19.0 LT	15+97.0	19.0 LT			25
	15+97.0	19.0 LT	15+99.0	19.0 LT			2
	15+99.0	19.0 LT	16+32.0	19.0 LT			33
	16+32.0	19.0 LT	16+34.0	19.0 LT			2
	16+34.0	19.0 LT	16+50.0	19.0 LT			16
	16+50.0	11.0 RT	14+24.0	11.0 RT			24
	14+24.0	11.0 RT	14+26.0	11.0 RT			2
	14+26.0	11.0 RT	14+48.5	11.0 RT			22.5
	14+48.5	11.0 RT	14+50.5	11.0 RT			2
	14+50.5	11.0 RT	14+83.0	11.0 RT			32.5
GNP-14	14+83.0	11.0 RT	14+85.0	11.0 RT			2
	14+85.0	11.0 RT	15+06.0	11.0 RT			21
	15+06.0	11.0 RT	15+08.0	11.0 RT			2
	15+08.0	11.0 RT	15+45.0	11.0 RT			37
	15+45.0	11.0 RT	15+47.0	11.0 RT			2
	15+47.0	11.0 RT	15+69.5	11.0 RT			22.5
	15+69.5	11.0 RT	15+71.5	11.0 RT			2
	15+71.5	11.0 RT	16+08.0	11.0 RT			34.5
	16+08.0	11.0 RT	16+10.0	11.0 RT			2
	16+10.0	11.0 RT	16+31.0	11.0 RT			21
	16+31.0	11.0 RT	16+33.0	11.0 RT			2
	16+33.0	11.0 RT	16+50.0	11.0 RT			17
GNP-15	16+50.0	19.0 LT	16+53.0	19.0 LT			3
	16+53.0	19.0 LT	16+61.0	19.0 LT			2
	16+61.0	19.0 LT	17+08.0	19.0 LT			48
	17+08.0	19.0 LT	17+41.8	48.2 LT	35	17+29.9	43
	17+41.8	48.2 LT	17+43.2	56.3 LT			8
	17+43.2	56.3 LT	17+98.5	22.4 LT	25	17+80.5	49
	17+98.5	22.4 LT	18+11.0	22.4 LT			11
	18+11.0	11.0 RT	16+97.3	11.0 RT			47
	16+97.3	11.0 RT	17+21.5	42.2 RT	25	17+16.6	46
	17+21.5	42.2 RT	17+55.7	38.8 RT			11
	17+55.7	38.8 RT	17+89.7	12.5 RT	35	17+68.2	46
	17+89.7	12.5 RT	18+11.0	12.5 RT			21
SUB TOTAL							1341
TOTAL							2777



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 Plattsburgh, New York 12901  
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 www.cscos.com



REGISTERED PROFESSIONAL ENGINEER



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 NEW YORK

COGAN AVENUE  
 RECONSTRUCTION  
 CITY OF PLATTSBURGH

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MISCELLANEOUS  
 TABLES

MST-01

SHEET 12 OF 59

 Mar 04, 2021 - 8:54am  
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**TABLE OF CONCRETE SIDEWALK**

ITEM 608.0101- CONCRETE SIDEWALK AND DRIVEWAYS

ITEM 608.20- SURFACE APPLIED DETECTABLE WARNING UNITS

	FROM STATION	TO STATION (SEE NOTE)	SIDE	LENGTH (FT)	WIDTH (FT)	THICKNESS (IN)	AREA (SF)	ITEM 608.20 (SY)	COMMENTS
GNP-01	1+01	1+16	LT	15	VARIES	4	107	1.2	CORNELIA (SE) CURB RAMP
	0+39	0+43	RT	4	VARIES	4	25	1.1	CORNELIA (NW) CURB RAMP
	1+00	1+14	RT	14	VARIES	4	116	2.6	CORNELIA (SW) CURB RAMP
GNP-03	5+50	5+62	LT	12	VARIES	4	64	0.9	TERRACE WEST WAY (SE) CURB RAMP
	5+58	6+47	LT	89	4	4	356		
	6+47	6+50	LT	3	4	6	12		DRIVEWAY 5
	5+45	5+62	RT	17	VARIES	4	65	1.4	TERRACE WEST WAY (SW) CURB RAMP
	5+62	5+78	RT	16	4	4	64		
	5+78	6+10	RT	32	4	6	128		DRIVEWAY 4
	6+10	6+50	RT	40	4	4	160		
GNP-04	6+50	6+70	LT	20	4	6	80		DRIVEWAY 5
	6+70	7+08	LT	38	4	4	152		
	7+08	7+23	LT	15	VARIES	4	59	1.2	HALEY DRIVE (NE) CURB RAMP
	7+79	7+91	LT	12	VARIES	4	44	1.2	HALEY DRIVE (SE) CURB RAMP
	7+91	9+00	LT	109	4	4	436		
	6+50	6+69	RT	19	4	4	80		
	6+69	6+87	RT	18	4	6	72		DRIVEWAY 6
	6+87	7+61	RT	74	4	4	296		
	7+61	7+73	RT	12	4	6	48		DRIVEWAY 7
	7+73	7+97	RT	24	4	4	96		
	7+97	8+21	RT	24	4	6	96		DRIVEWAY 8
	8+21	8+92	RT	71	4	4	284		
	8+92	9+00	RT	8	4	6	32		DRIVEWAY 9
GNP-05	9+00	9+14	LT	14	4	4	56		
	9+14	9+30	LT	16	4	6	64		DRIVEWAY 10
	9+30	9+96	LT	66	4	4	264		
	9+96	10+13	LT	17	4	6	68		DRIVEWAY 11
	10+13	10+32	LT	19	4	4	76		
	10+32	10+52	LT	20	4	6	80		DRIVEWAY 13
	10+52	11+50	LT	98	4	4	392		
	9+00	9+12	RT	12	4	6	48		DRIVEWAY 9
	9+12	10+27	RT	115	4	4	460		
	10+27	10+50	RT	23	4	6	92		DRIVEWAY 12
10+50	10+65	RT	15	4	4	60			
10+65	10+89	RT	24	4	6	96		DRIVEWAY 14	
10+89	11+50	RT	61	4	4	244			
GNP-06	11+50	11+57	LT	7	4	4	28		
	11+57	11+67	LT	10	4	6	40		DRIVEWAY 15
	11+67	12+23	LT	56	4	4	224		
	12+23	12+35	LT	12	4	6	48		DRIVEWAY 17
	12+35	12+85	LT	50	4	4	200		
	12+85	12+85	LT	10	4	6	40		DRIVEWAY 19
	12+85	13+46	LT	51	4	4	204		
	13+46	13+56	LT	10	4	6	40		DRIVEWAY 21
	13+56	14+00	LT	44	4	4	176		
	11+50	11+97	RT	47	4	4	188		
	11+97	12+09	RT	12	4	6	48		DRIVEWAY 16
	12+09	12+44	RT	35	4	4	140		
	12+44	12+65	RT	21	4	6	84		DRIVEWAY 18
12+65	13+15	RT	50	4	4	200			
13+15	13+25	RT	10	4	6	40		DRIVEWAY 20	
13+25	13+71	RT	46	4	4	184			
13+71	13+73	RT	12	4	6	48		DRIVEWAY 22	
13+73	14+00	RT	17	4	4	68			
<b>SUB TOTAL</b>								<b>9.6</b>	

**TABLE OF CONCRETE SIDEWALK (CONTINUED)**

ITEM 608.0101- CONCRETE SIDEWALK AND DRIVEWAYS

ITEM 608.20- SURFACE APPLIED DETECTABLE WARNING UNITS

	FROM STATION	TO STATION (SEE NOTE)	SIDE	LENGTH (FT)	WIDTH (FT)	THICKNESS (IN)	AREA (SF)	ITEM 608.20 (SY)	COMMENTS
GNP-07	14+00	14+03	LT	3	4	4	12		
	14+03	14+15	LT	11	4	6	44		DRIVEWAY 23
	14+15	14+68	LT	53	4	4	212		
	14+68	14+79	LT	11	4	6	44		DRIVEWAY 25
	14+79	15+28	LT	49	4	4	196		
	15+28	15+38	LT	10	4	6	40		DRIVEWAY 27
	15+38	15+79	LT	41	4	4	164		
	15+79	15+96	LT	17	4	6	68		DRIVEWAY 29
	15+96	16+40	LT	44	4	4	176		
	16+40	16+50	LT	10	4	6	40		DRIVEWAY 31
	14+00	14+32	RT	32	4	4	128		
	14+32	14+42	RT	10	4	6	40		DRIVEWAY 24
	14+42	14+89	RT	47	4	4	188		
15+02	15+54	RT	52	4	4	208			
15+54	15+65	RT	11	4	6	44		DRIVEWAY 28	
15+65	16+15	RT	50	4	4	200			
16+15	16+25	RT	11	4	6	44		DRIVEWAY 30	
16+25	16+50	RT	25	4	4	100			
GNP-08	16+50	16+54	LT	4	4	6.0	16		DRIVEWAY 31
	16+54	17+12	LT	58	4	4.0	232		
	17+12	17+27	LT	15	VARIES	4.0	54	1.5	PARK AVE WEST (NE) CURB RAMP
	16+50	16+99	RT	49	4	4.0	196		
	16+99	17+13	RT	14	VARIES	4.0	52	1.3	PARK AVE WEST (NW) CURB RAMP
<b>SUB TOTAL</b>								<b>2.8</b>	NOTE: STATION REFERS TO FARTHEST POINT ON SIDEWALK LIMIT.
<b>TOTAL</b>								<b>12.4</b>	



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**COGAN AVENUE  
RECONSTRUCTION**  
**CITY OF PLATTSBURGH**

Mer 04, 2021 - 8:54am F:\Project\A54 - City of Plattsburgh\A54003001 - Cogan Avenue\Design\CADD\Sheet Files\MST\_02.dwg

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S.GALLAGHER		
DESIGNED BY: K.MCARDELL		
CHECKED BY: T.HUMPHREY		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

**MISCELLANEOUS  
TABLES**

**MST-02**

SHEET 13 OF 59

TABLE OF DRIVEWAYS					
ITEM 608.020102- HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS, BICYCLE PATHS, AND VEGETATION CONTROL STRIPS					
ITEM 608.03040008- REMOVE, STORE AND RESET CONCRETE BLOCK PAVERS					
DRIVEWAY NUMBER	STATION	SIDE	LENGTH (FT)	WIDTH (FT)	COMMENTS
1	3+58	LT	4.6	17.9	PAVER RESET
2	3+79	RT	14.5	17.8	
3	5+33	LT	17.0	11.0	
4	5+99	RT	18.9	31.6	
5	6+57	LT	18.1	22.1	
6	6+79	RT	17.7	17.6	
7	7+67	RT	17.6	12.1	WIDE APRON
8	8+10	RT	17.5	24.3	
9	9+03	RT	17.4	19.4	
10	9+22	LT	18.7	15.6	
11	10+04	LT	18.4	17.6	
12	10+38	RT	17.7	23.4	
13	10+42	LT	18.7	19.6	
14	10+78	RT	17.9	23.7	
15	11+63	LT	17.9	10.8	
16	12+05	RT	18.8	12.1	
17	12+29	LT	17.4	11.8	
18	12+57	RT	19.1	21.2	
19	12+91	LT	17.8	9.5	
20	13+21	RT	17.9	10.2	WIDE APRON
21	13+52	LT	18.4	9.9	
22	13+78	RT	17.6	11.6	
23	14+09	LT	18.4	11.3	
24	14+35	RT	17.6	10.6	WIDE APRON
25	14+72	LT	18.5	11.0	
26	14+96	RT	11.4	13.0	PAVER RESET
27	15+31	LT	18.6	10.0	
28	15+60	RT	17.5	10.4	WIDE APRON
29	15+85	LT	18.6	16.9	
30	16+21	RT	17.4	10.4	WIDE APRON
31	16+47	LT	18.7	14.9	

TABLE OF SEDIMENT FILTER LOG			
ITEM 209.2301 - SEDIMENT FILTER LOG - TEMPORARY, 12" (LF)			
FROM STATION	TO STATION	SIDE	QUANTITY (LF)
4+81	4+98	RT	50
7+12	7+30	LT	35
7+75	7+86	LT	30
13+28	13+72	RT	45
13+85	14+31	RT	50
16+27	17+11	RT	85
TOTAL:			295

TABLE OF ALTERING SANITARY MANHOLES		
ITEM 604.070701 - ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES (EA)		
STATION	SIDE	QUANTITY (EA)
4+14	LT	1
4+82	LT	1
5+21	LT	1
8+56	LT	1
11+54	LT	1
14+72	LT	1
17+63	LT	1
TOTAL		7

TABLE OF INLET PROTECTION		
ITEM 209.1703 - DRAINAGE STRUCTURE INLET PROTECTION, PREFABRICATED - TEMPORARY (LF)		
STRUCTURE ID	QUANTITY (LF)	
DS 2-2	15	
DS 2-3	15	
DS 2-6	15	
DS 2-7	15	
DS 3-2	15	
DS 3-3	15	
DS 3-4	15	
DS 4-2	15	
DS 4-4	15	
DS 4-5	15	
DS 4-6	15	
DS 4-9	15	
DS 4-10	15	
DS 5-3	15	
DS 5-4	15	
DS 6-2	15	
DS 6-3	15	
DS 7-2	15	
DS 7-3	15	
DS 7-5	15	
DS 7-6	15	
DS 8-4	15	
DS 8-6	15	
TOTAL:		345



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**COGAN AVENUE  
RECONSTRUCTION**  
CITY OF PLATTSBURGH

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S.GALLAGHER		
DESIGNED BY: K.MCARDELL		
CHECKED BY: T.HUMPHREY		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

MISCELLANEOUS TABLES

**MST-03**

SHEET 14 OF 59

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**COGAN AVENUE  
 RECONSTRUCTION**  
 CITY OF PLATTSBURGH

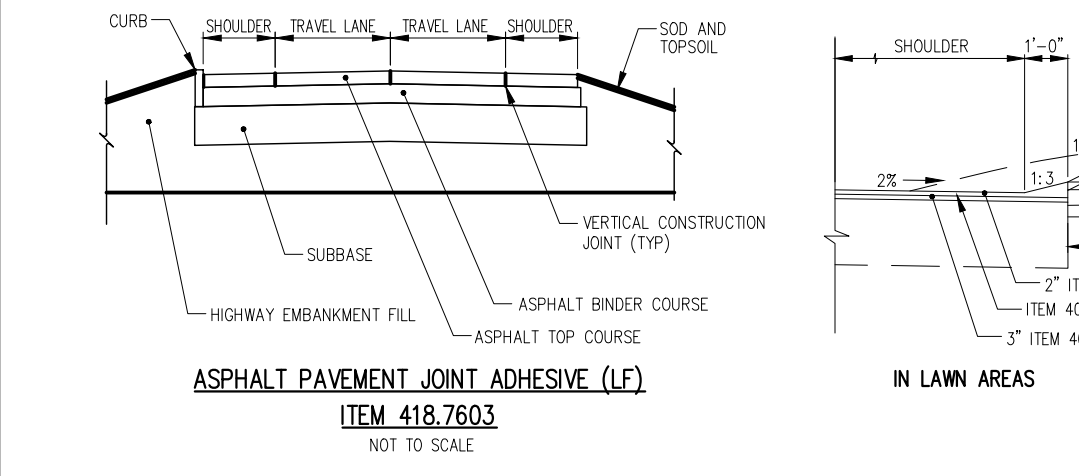
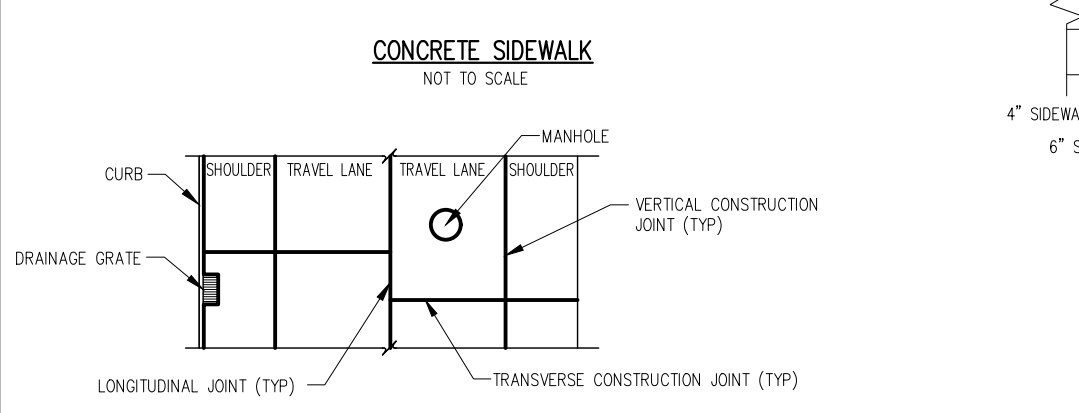
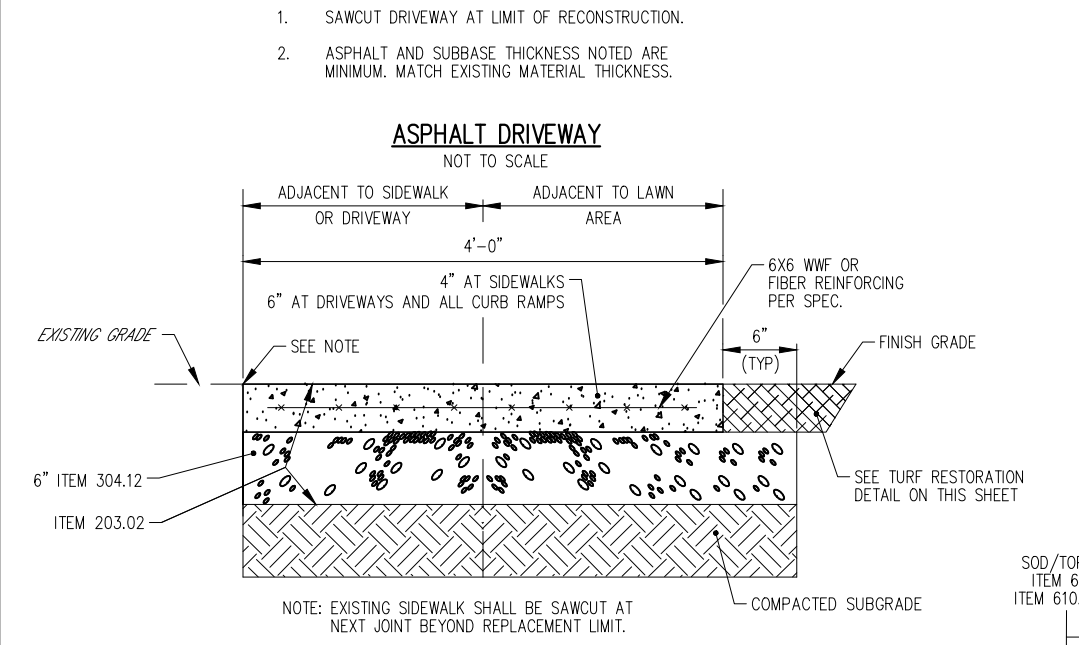
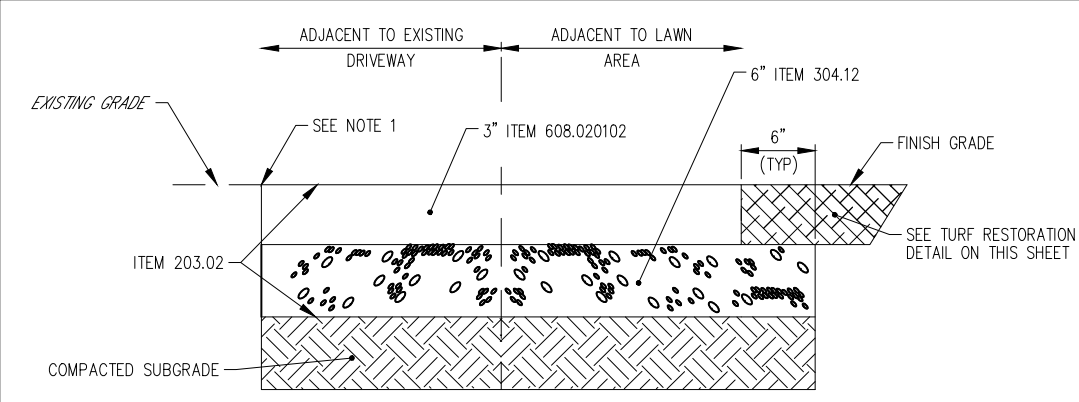
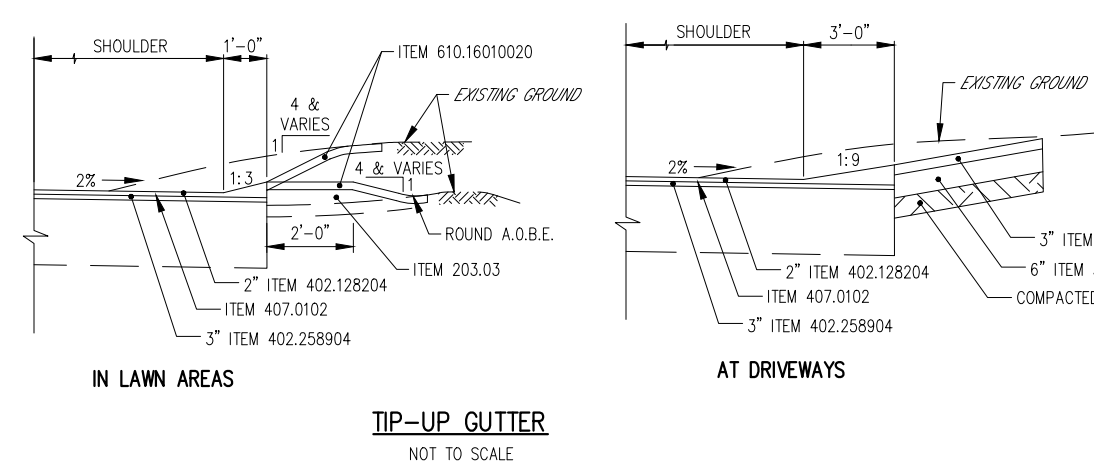
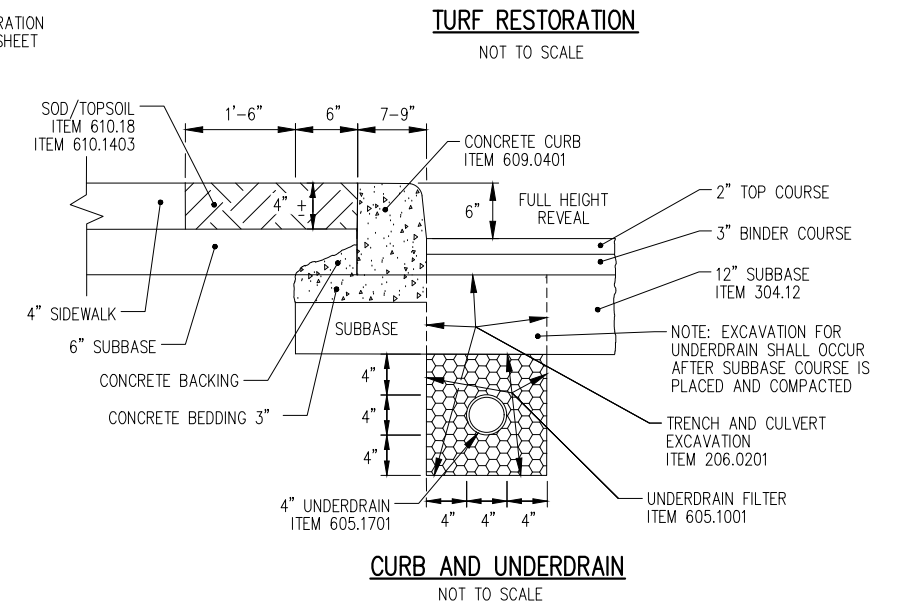
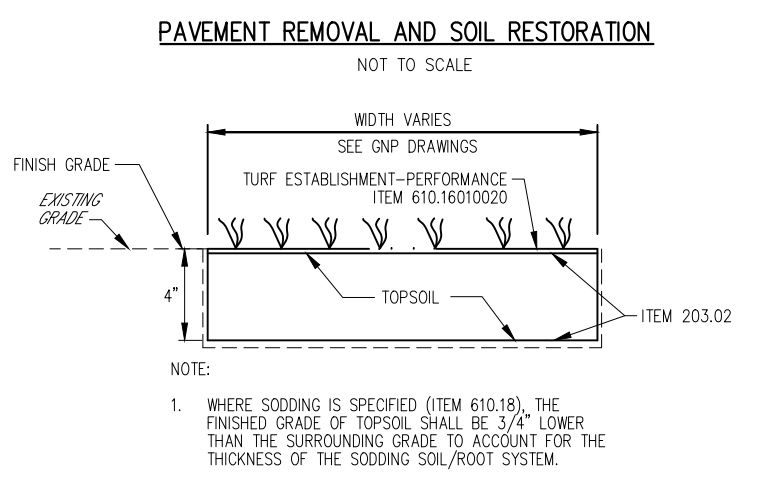
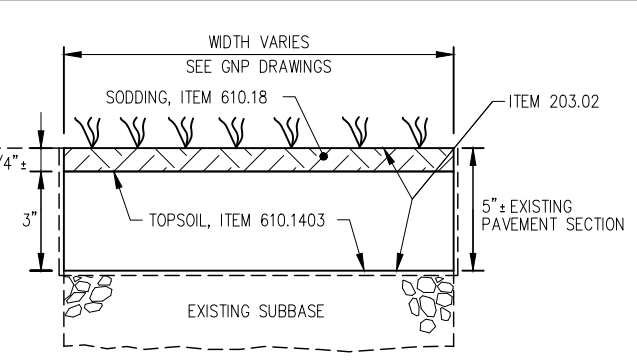
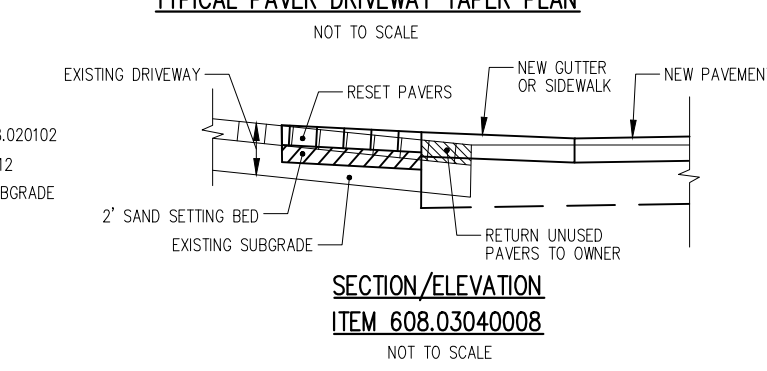
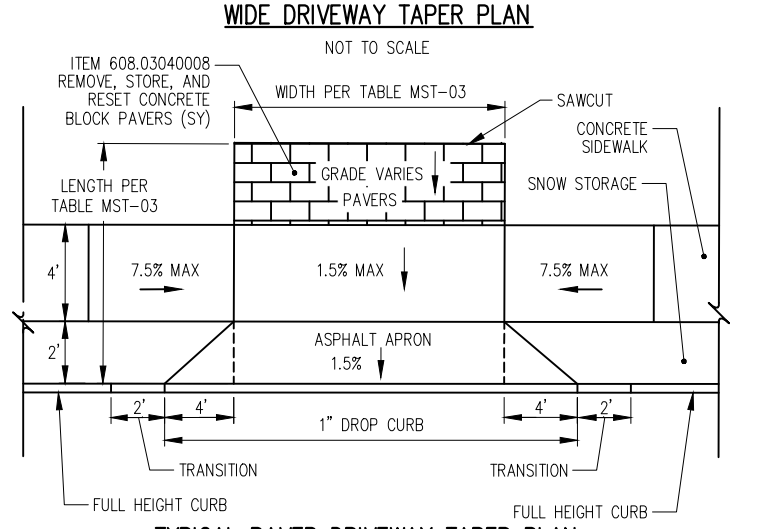
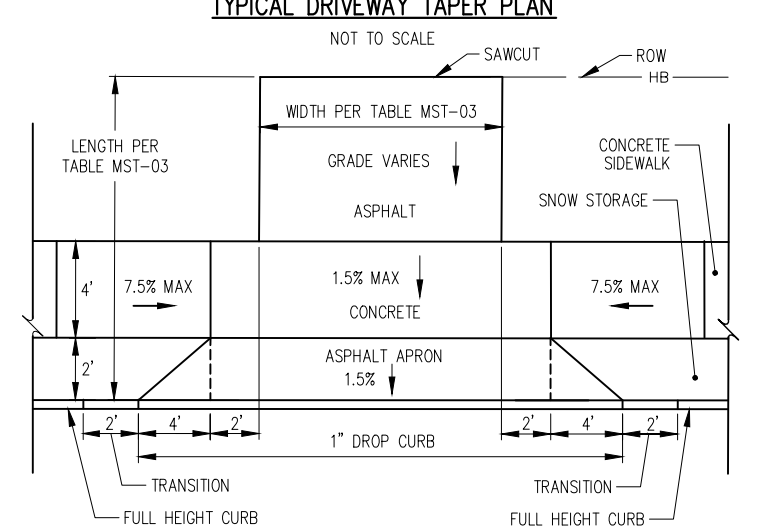
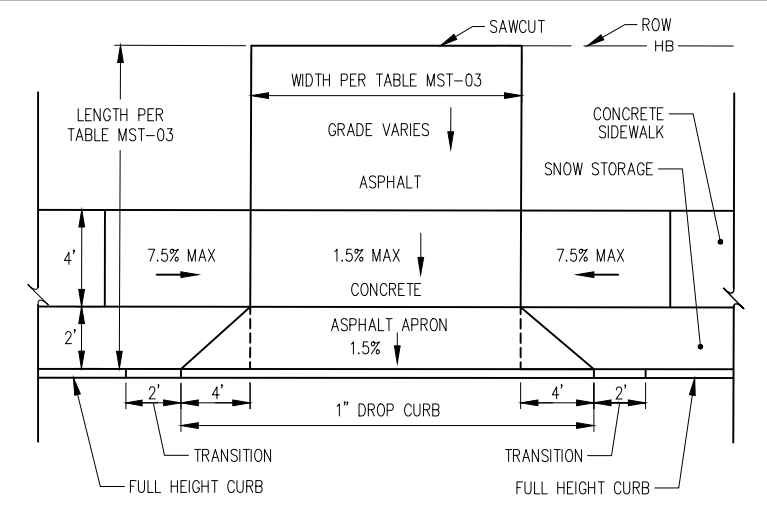
MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S.GALLAGHER		
DESIGNED BY: K.MCARDELL		
CHECKED BY: T.HUMPHREY		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

**MISCELLANEOUS  
 DETAILS**

**MSD-01**

SHEET 15 OF 59

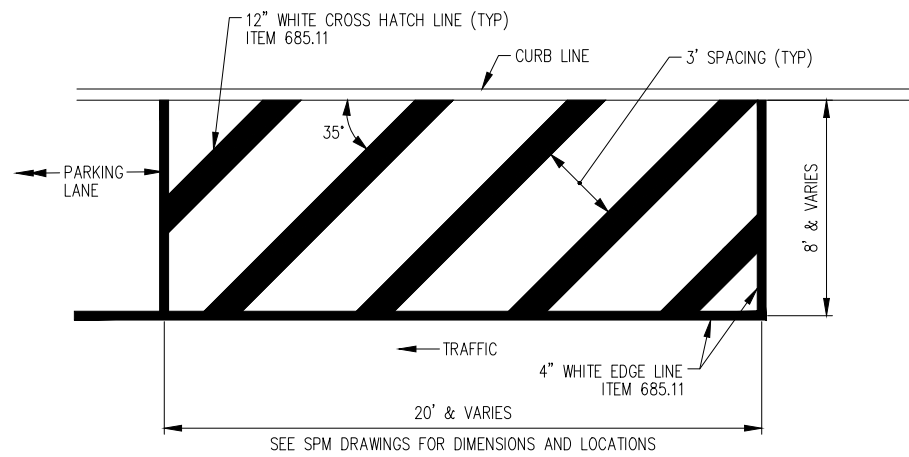
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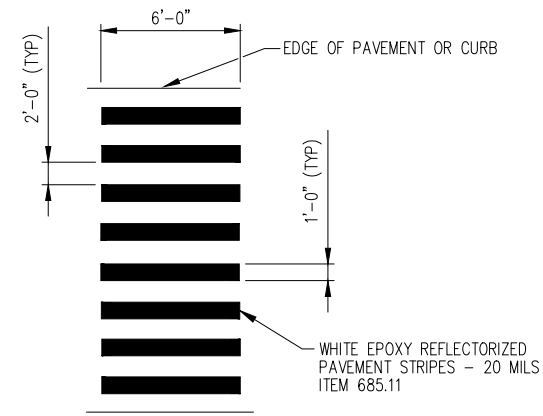
1. SAWCUT DRIVEWAY AT LIMIT OF RECONSTRUCTION.
2. ASPHALT AND SUBBASE THICKNESS NOTED ARE MINIMUM. MATCH EXISTING MATERIAL THICKNESS.

NOTE: EXISTING SIDEWALK SHALL BE SAWCUT AT NEXT JOINT BEYOND REPLACEMENT LIMIT.

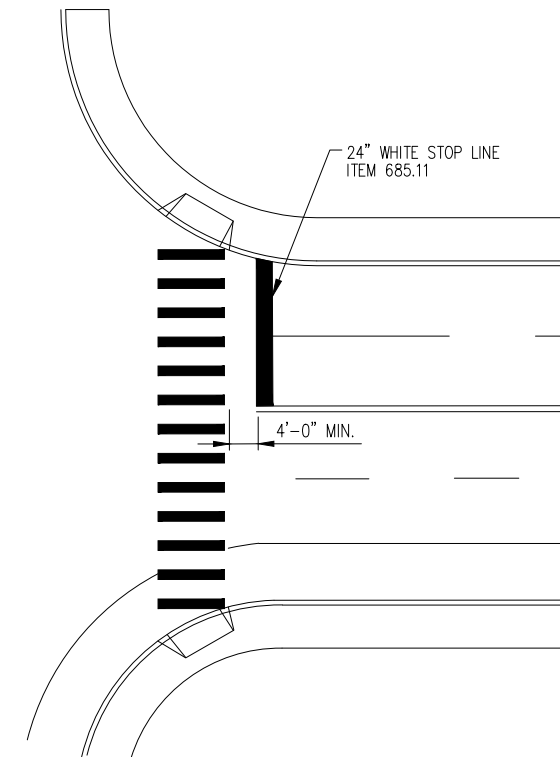
NOTE: EXCAVATION FOR UNDERDRAIN SHALL OCCUR AFTER SUBBASE COURSE IS PLACED AND COMPACTED



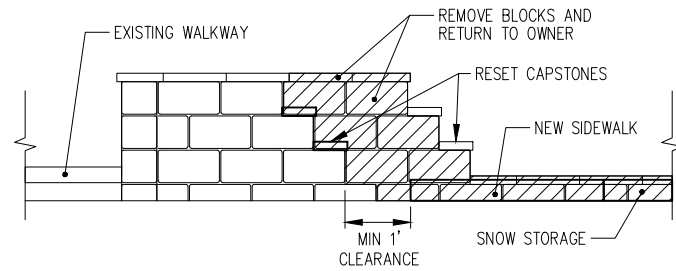
**PARKING LANE HATCHING**  
NOT TO SCALE



**TYPICAL TYPE L CROSSWALK**  
NOT TO SCALE



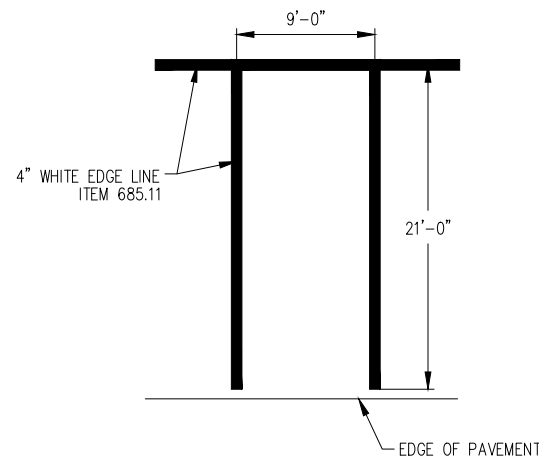
**TYPICAL STOP BAR**  
NOT TO SCALE



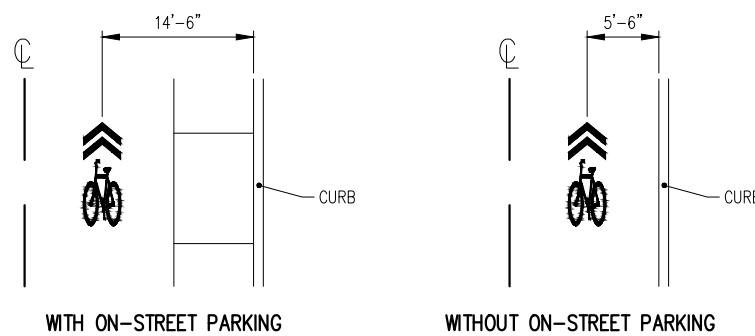
**DRIVEWAY D6 BLOCK WALL REMOVAL**  
NOT TO SCALE

**NOTES:**

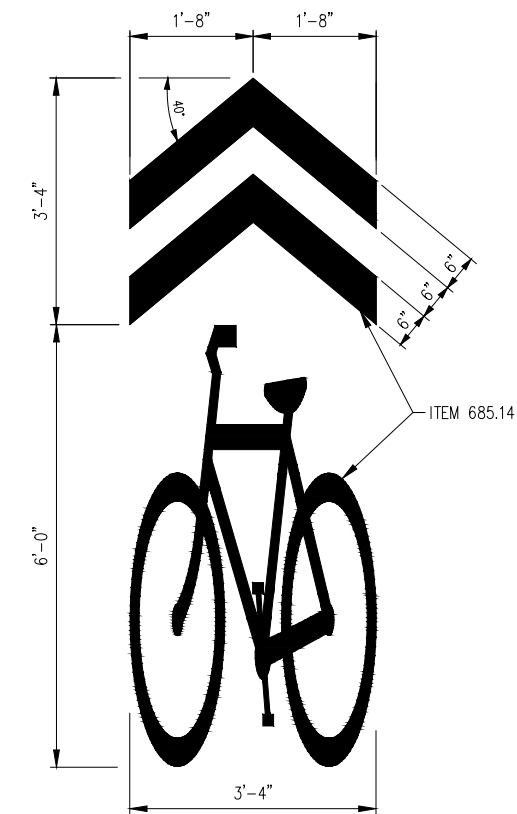
1. REMOVE SHADED BLOCKS AND RETURN TO THE HOMEOWNER.
2. GRADE THE LAWN BACK TO MEET THE NEW SIDEWALK.
3. RESET THE CAPSTONES ON THE EDGE OF THE WALL.



**TYPICAL PULL-IN PARKING SPACES**  
NOT TO SCALE



**SHARED LANE MARKING SYMBOL PLACEMENT**  
NOT TO SCALE



**SHARED LANE MARKING SYMBOL**  
NOT TO SCALE



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**COGAN AVENUE  
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CITY OF PLATTSBURGH

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO:		A54.003.001
DATE:		MARCH 2021
DRAWN BY:		S.GALLAGHER
DESIGNED BY:		K.MCARDELL
CHECKED BY:		T.HUMPHREY
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

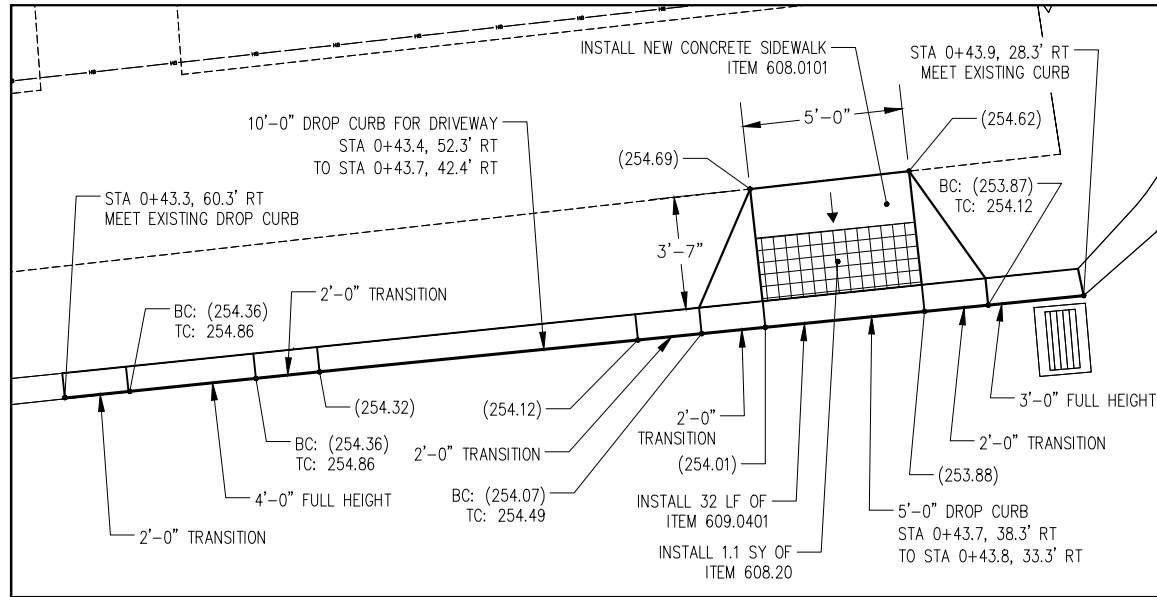
**MISCELLANEOUS  
DETAILS**

**MSD-02**

SHEET 16 OF 59

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**CORNELIA STREET AND COGAN AVENUE  
(NORTHWEST QUADRANT)**

SEE DWG NO. GNP-01

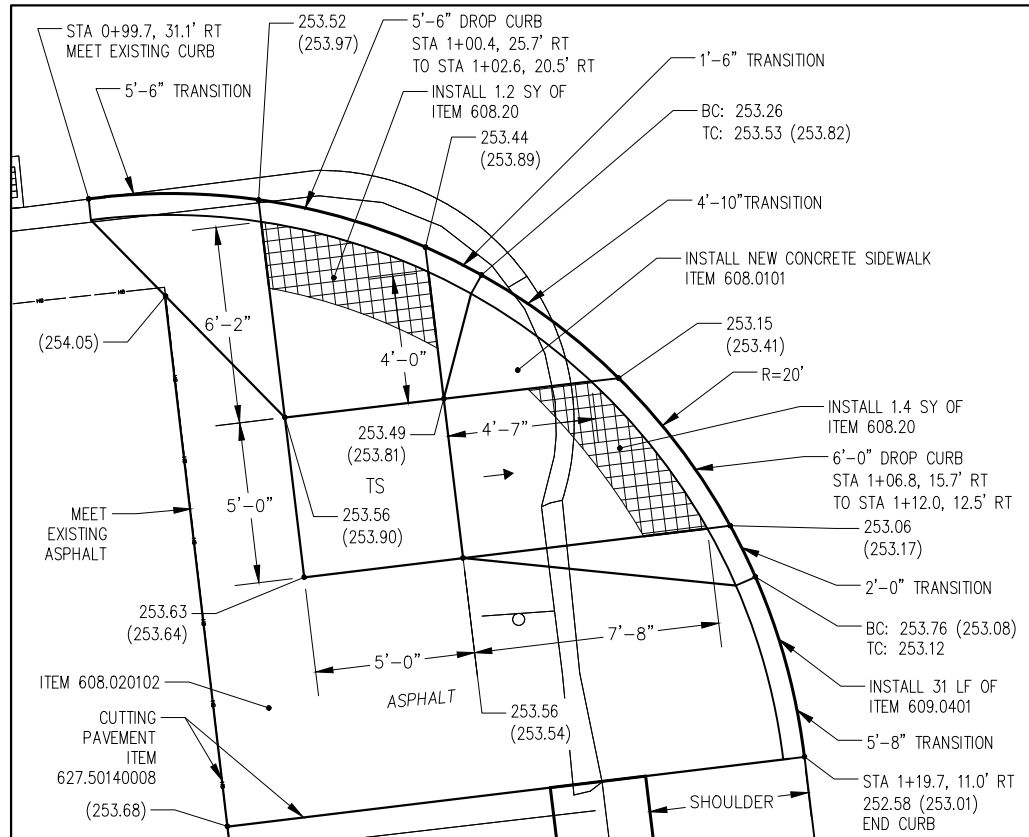
NOTES:

1. THE DETAILS AND DIMENSIONS PROVIDED ARE APPROXIMATE TO ACHIEVE ADA COMPLIANT RAMPS. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES TO ENSURE COMPLIANCE WITH ADA REQUIREMENTS. CROSS SLOPE ON ALL SIDEWALKS AND RAMPS SHALL NOT EXCEED 1.5%.
2. IF LAYOUTS REQUIRE ADJUSTMENT DUE TO FIELD CONDITIONS, NOTIFY ENGINEER IMMEDIATELY.
3. SIDEWALK REMOVALS SHALL END AT AN EXISTING JOINT.
4. REFER TO NYSDOT STANDARD SHEET 608-01 ISSUED UNDER EI 20-005 FOR ADDITIONAL CURB RAMP NOTES AND DETAILS.
5. ARROWS ON RAMPS REPRESENT DOWN GRADE DIRECTION. THE RUNNING SLOPE OF A CURB RAMP SHALL BE 1:20 (5%) MINIMUM AND 1:13.33 (7.5%) MAXIMUM.
6. REFER TO DWG NO. MSD-01 FOR CURB & SIDEWALK INSTALLATION DETAILS.
7. THE MAXIMUM CROSS SLOPE FOR CROSSWALKS SHALL BE 1.5% AT INTERSECTIONS WITH YIELD OR STOP CONTROL, AND 4.5% AT SIGNALIZED INTERSECTIONS.

ITEM NO	DESCRIPTION	UNIT
608.0101	CONCRETE SIDEWALKS	CY
608.020102	HMA SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS	TON
608.20	SURFACE APPLIED DETECTABLE WARNING UNITS	SY
609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF150	LF

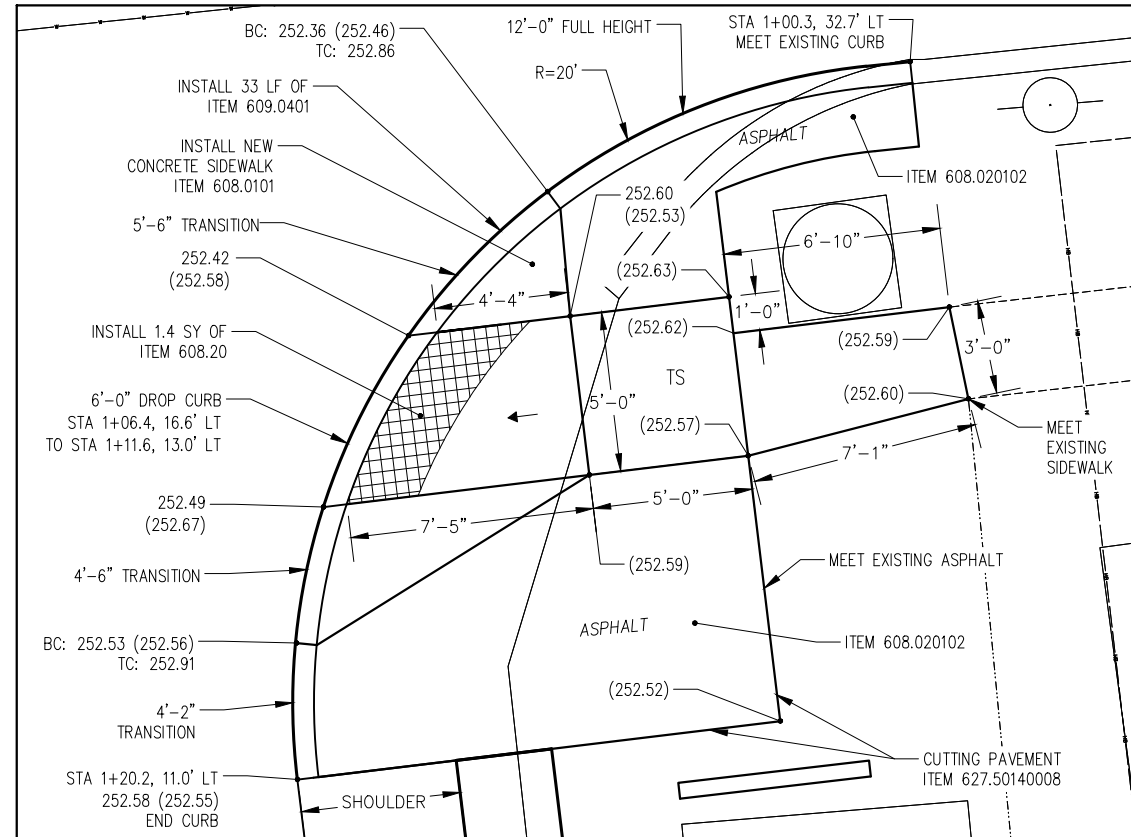
LEGEND

- ← CURB RAMP
- NEW CURB
- ▨ DETECTABLE WARNING UNIT
- ▭ NEW SIDEWALK (ITEM 608.0101)
- 590.00 PROPOSED ELEVATION
- (590.00) EXISTING ELEVATION
- TS TURNING SPACE



**CORNELIA STREET AND COGAN AVENUE  
(SOUTHWEST QUADRANT)**

SEE DWG NO. GNP-01



**CORNELIA STREET AND COGAN AVENUE  
(SOUTHEAST QUADRANT)**

SEE DWG NO. GNP-01



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Phone: 315-455-2000  
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**COGAN AVENUE  
RECONSTRUCTION**  
CITY OF PLATTSBURGH

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: M. GROSS		
DESIGNED BY: M. GROSS		
CHECKED BY: T. HUMPHREY		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

**CURB RAMP  
DETAILS**

**CRD-01**

SHEET 17 OF 59



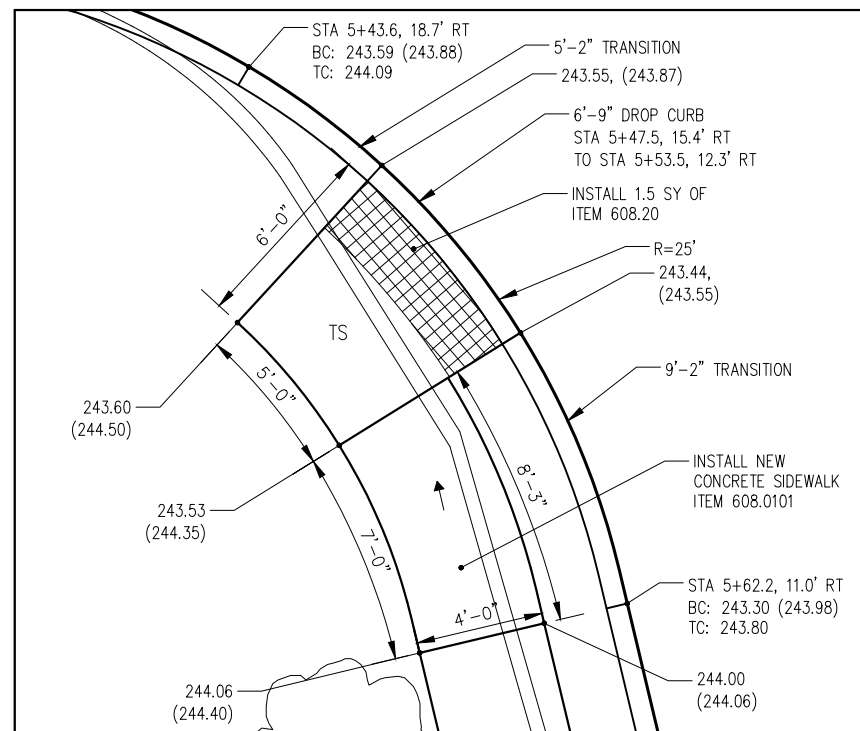
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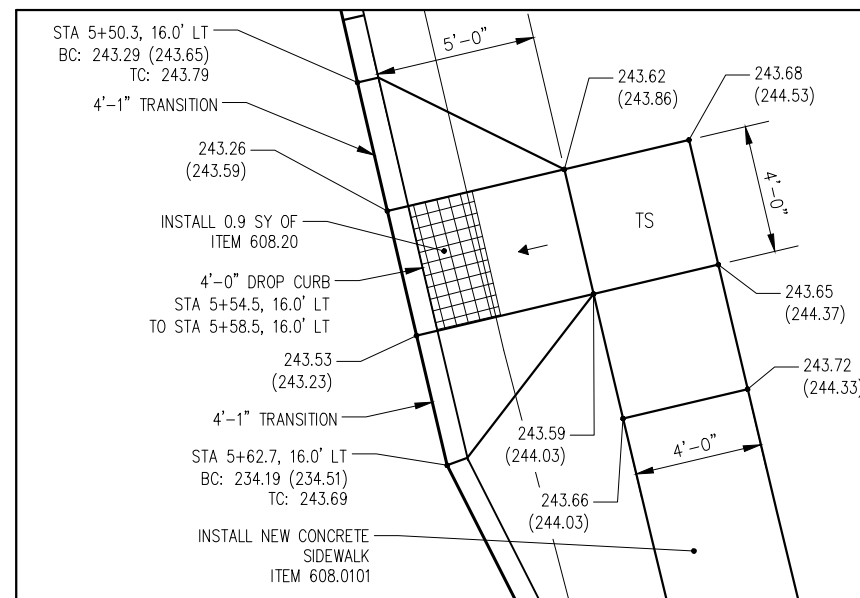
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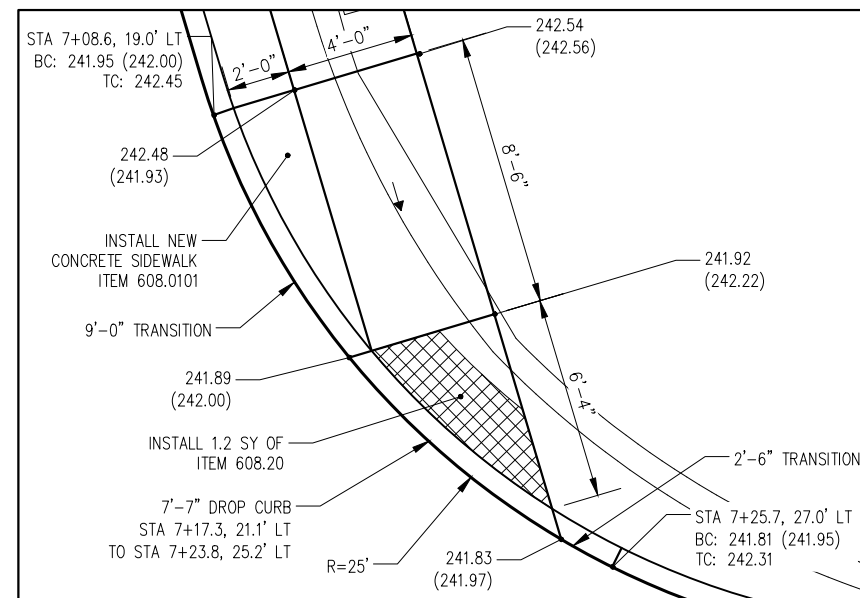
**COGAN AVENUE  
RECONSTRUCTION**  
CITY OF PLATTSBURGH



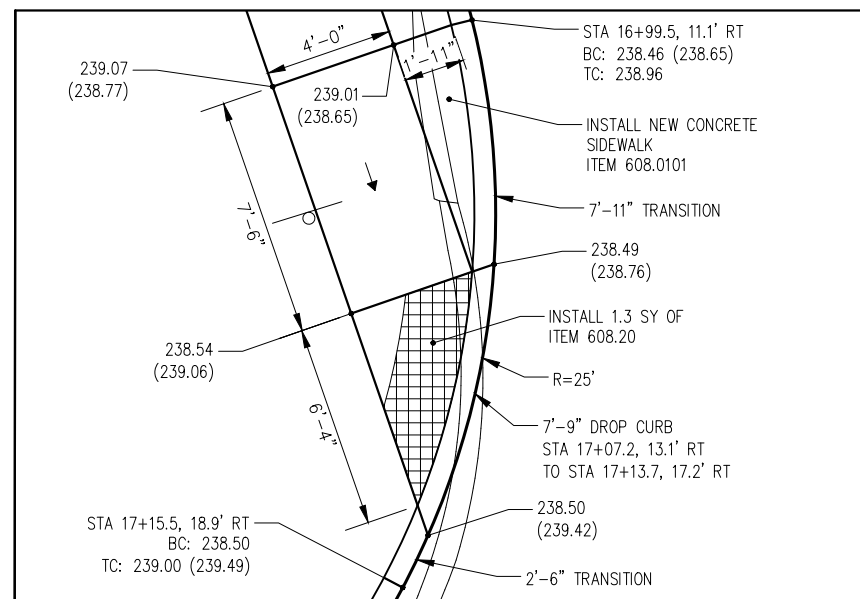
**COGAN AVENUE AND TERRACE WEST WAY  
(SOUTHWEST QUADRANT)**  
SEE DWG NO. GNP-03



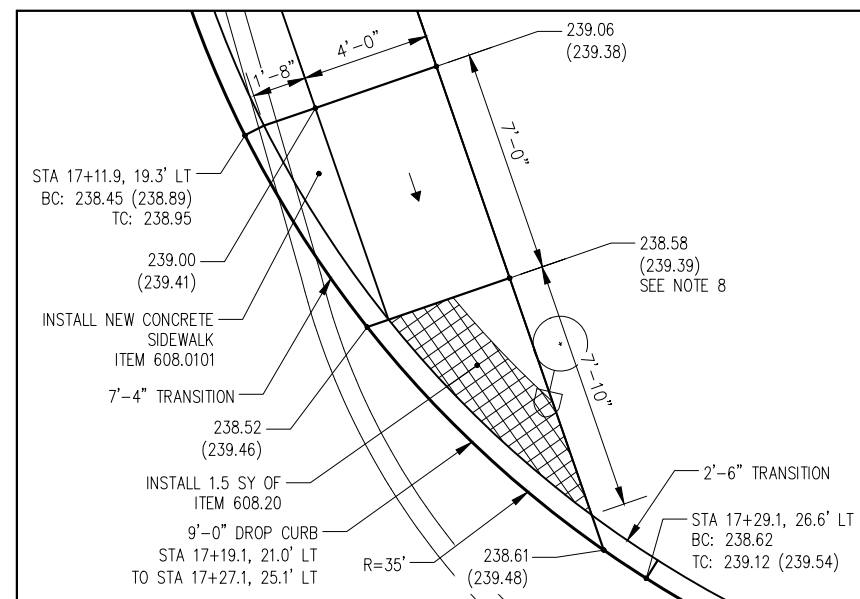
**COGAN AVENUE AND TERRACE WEST WAY  
(SOUTHEAST QUADRANT)**  
SEE DWG NO. GNP-03



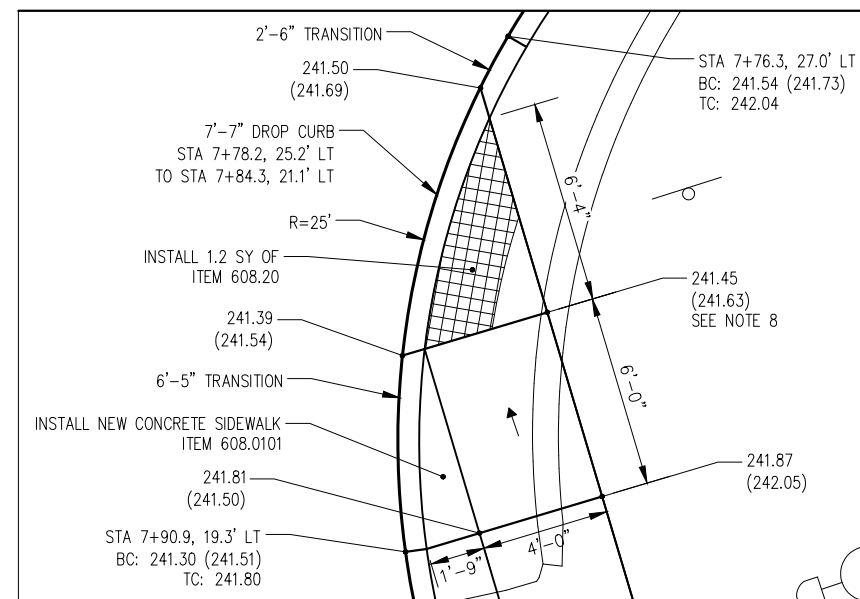
**COGAN AVENUE AND HALEY DRIVE  
(NORTHEAST QUADRANT)**  
SEE DWG NO. GNP-04



**COGAN AVENUE AND PARK AVENUE WEST  
(NORTHWEST QUADRANT)**  
SEE DWG NO. GNP-08



**COGAN AVENUE AND PARK AVENUE WEST  
(NORTHEAST QUADRANT)**  
SEE DWG NO. GNP-08



**COGAN AVENUE AND HALEY DRIVE  
(SOUTHEAST QUADRANT)**  
SEE DWG NO. GNP-04

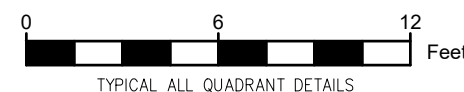
**LEGEND**

- ← CURB RAMP
- NEW CURB
- ▨ DETECTABLE WARNING UNIT
- NEW SIDEWALK (ITEM 608.0101)
- 590.00 PROPOSED ELEVATION
- (590.00) EXISTING ELEVATION
- TS TURNING SPACE

ITEM NO	DESCRIPTION	UNIT
608.0101	CONCRETE SIDEWALKS	CY
608.20	SURFACE APPLIED DETECTABLE WARNING UNITS	SY
609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF150	LF

**NOTES:**

- THE DETAILS AND DIMENSIONS PROVIDED ARE APPROXIMATE TO ACHIEVE ADA COMPLIANT RAMPS. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES TO ENSURE COMPLIANCE WITH ADA REQUIREMENTS. CROSS SLOPE ON ALL SIDEWALKS AND RAMPS SHALL NOT EXCEED 1.5%.
- IF LAYOUTS REQUIRE ADJUSTMENT DUE TO FIELD CONDITIONS, NOTIFY ENGINEER IMMEDIATELY.
- SIDEWALK REMOVALS SHALL END AT AN EXISTING JOINT.
- REFER TO NYSDOT STANDARD SHEET 608-01 ISSUED UNDER EB 16-012 FOR ADDITIONAL CURB RAMP NOTES AND DETAILS.
- ARROWS ON RAMPS REPRESENT DOWN GRADE DIRECTION. THE RUNNING SLOPE OF A CURB RAMP SHALL BE 1:20 (5%) MINIMUM AND 1:13.33 (7.5%) MAXIMUM.
- REFER TO DWG NO. MSD-01 FOR CURB & SIDEWALK INSTALLATION DETAILS.
- THE MAXIMUM CROSS SLOPE FOR CROSSWALKS SHALL BE 1.5% AT INTERSECTIONS WITH YIELD OR STOP CONTROL, AND 4.5% AT SIGNALIZED INTERSECTIONS.
- CONTRACTOR SHALL FIELD ADJUST ELEVATION AS REQUIRED TO PREVENT DRAINAGE BACKFLOW ONTO RAMP WHILE MAINTAINING TOLERANCE FOR MAXIMUM CROSS SLOPE.



GRID NORTH



Mar 04, 2021 - 8:55am F:\Project\A54 - City of Plattsburgh\A54003001 - Cogan Avenue\Design\CADD\Sheet Files\CRD\_02.dwg

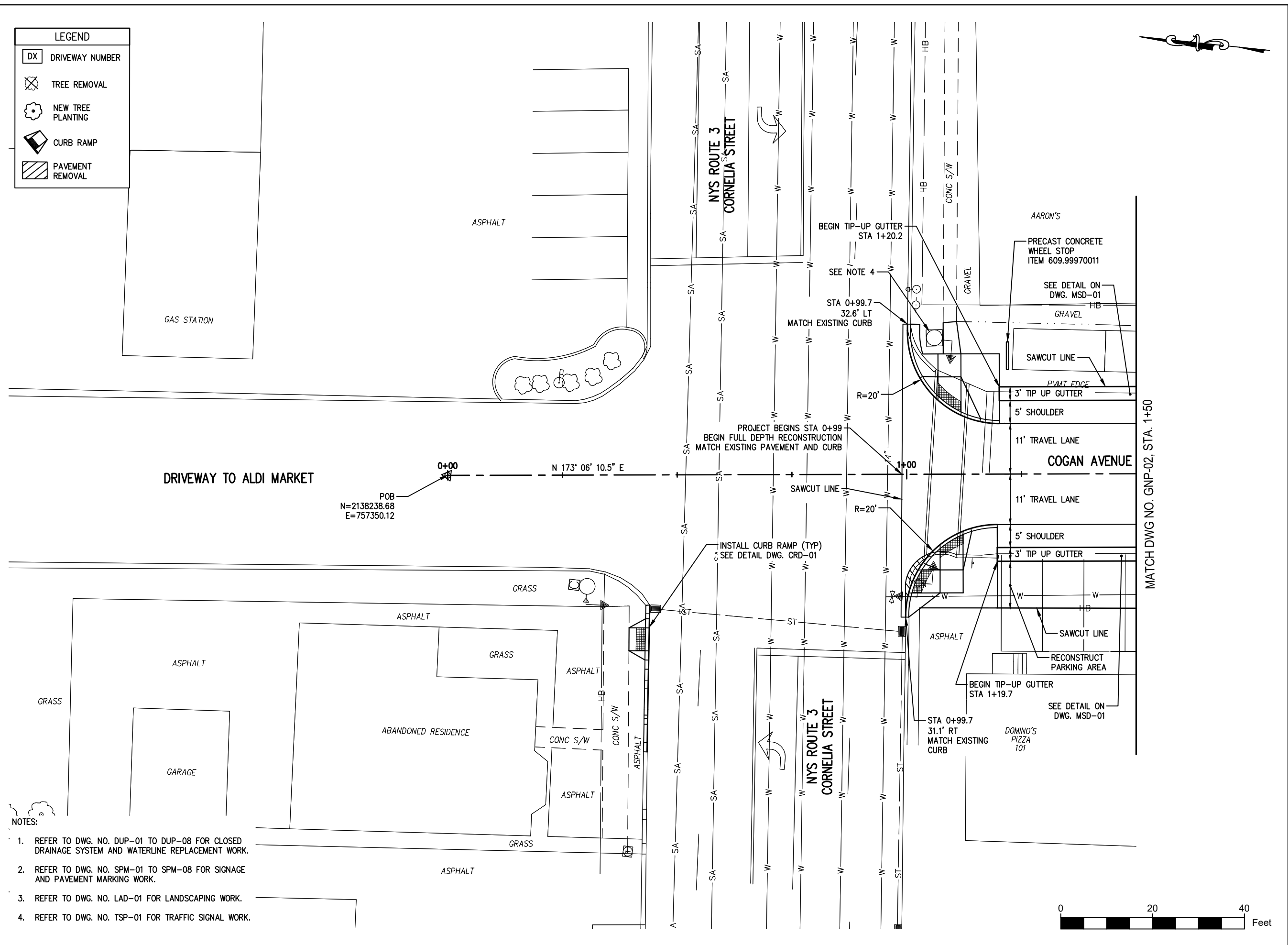
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REVISIONS		
		PROJECT NO: A54.003.001
		DATE: MARCH 2021
		DRAWN BY: M. GROSS
		DESIGNED BY: M. GROSS
		CHECKED BY: T. HUMPHREY

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**CURB RAMP  
DETAILS**

**CRD-02**

LEGEND	
DX	DRIVEWAY NUMBER
	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL



DRIVEWAY TO ALDI MARKET

POB  
N=2138238.68  
E=757350.12

N 173° 06' 10.5" E

MATCH DWG NO. GNP-02, STA. 1+50

- NOTES:
- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
  - REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
  - REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.
  - REFER TO DWG. NO. TSP-01 FOR TRAFFIC SIGNAL WORK.



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**COGAN AVENUE  
RECONSTRUCTION**  
CITY OF PLATTSBURGH

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S. GALLAGHER		
DESIGNED BY: K. MCARDELL		
CHECKED BY: T. HUMPHREY		
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**GENERAL  
PLAN**

**GNP-01**

SHEET 19 OF 59

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MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
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**GENERAL  
PLAN**

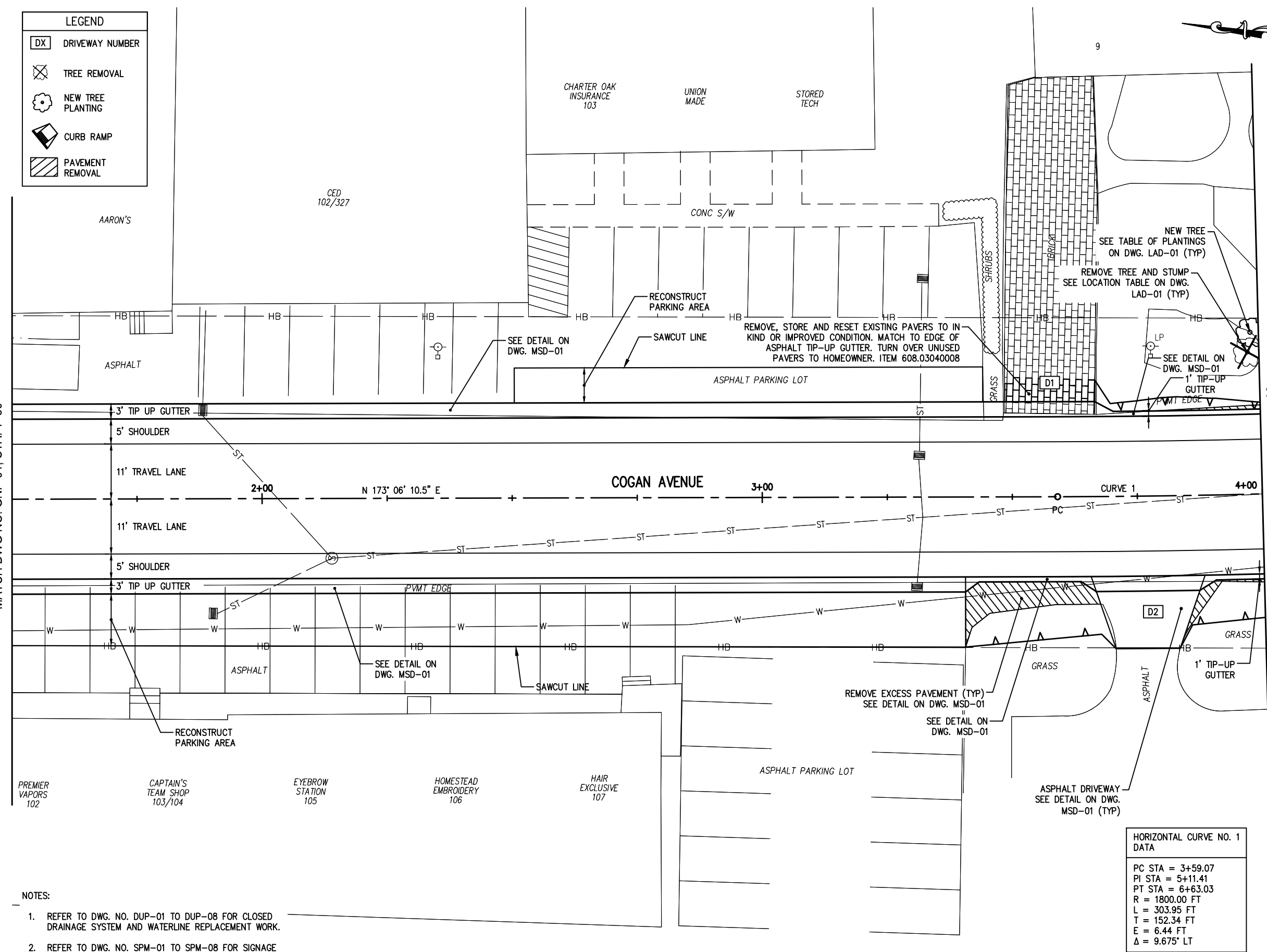
**GNP-02**

SHEET 20 OF 59

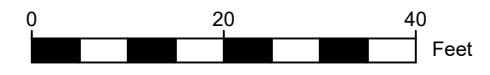
LEGEND	
	DRIVEWAY NUMBER
	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL

MATCH DWG NO. GNP-01, STA. 1+50

MATCH DWG NO. GNP-03, STA. 4+00



HORIZONTAL CURVE NO. 1 DATA	
PC STA	= 3+59.07
PI STA	= 5+11.41
PT STA	= 6+63.03
R	= 1800.00 FT
L	= 303.95 FT
T	= 152.34 FT
E	= 6.44 FT
Δ	= 9.675° LT



- NOTES:
- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
  - REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
  - REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.

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RECONSTRUCTION**  
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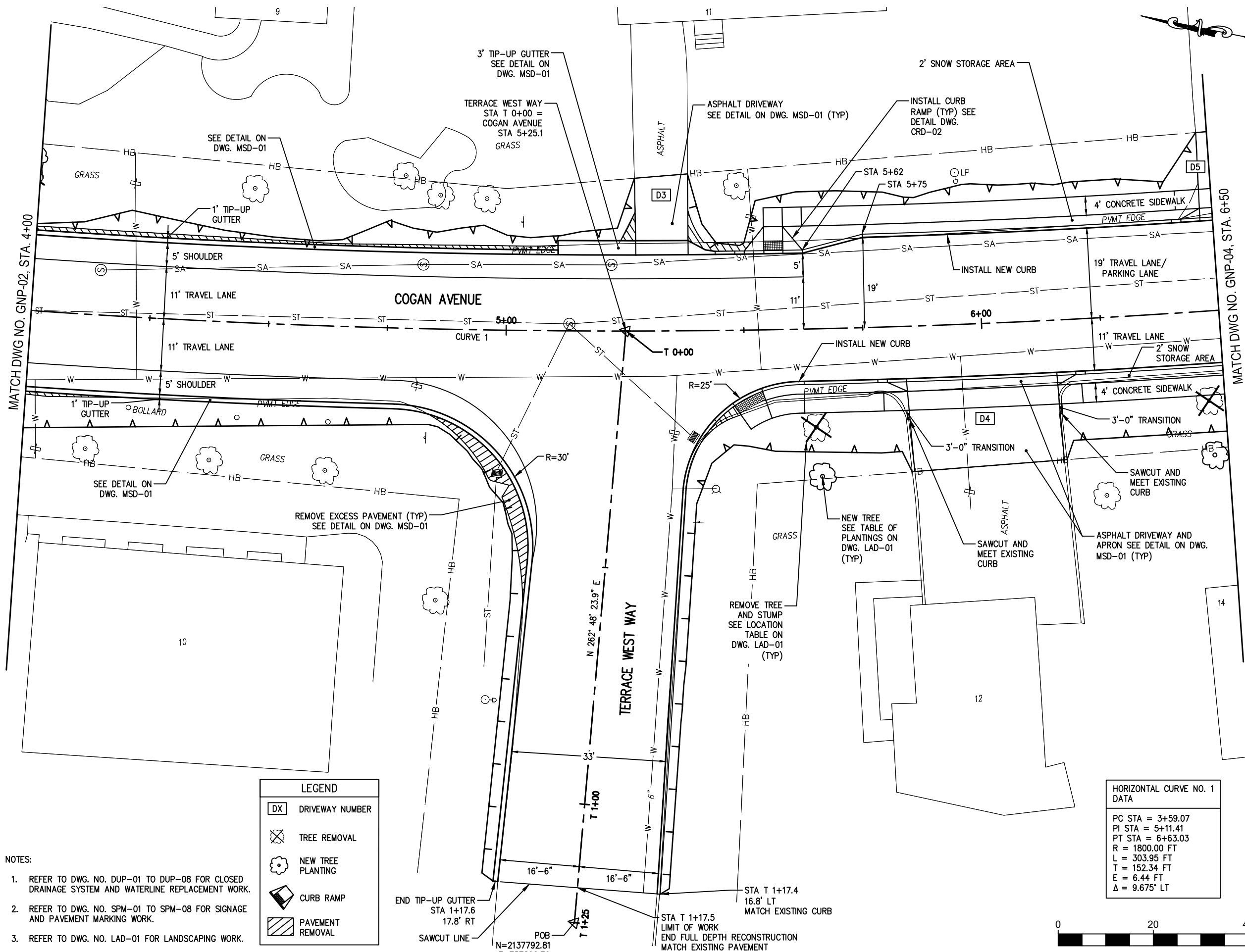
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MARK	DATE	DESCRIPTION
REVISIONS		
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**GENERAL  
PLAN**

**GNP-03**



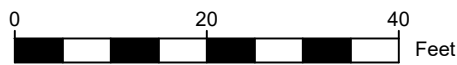
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	DRIVEWAY NUMBER
	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL

- NOTES:**
- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
  - REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
  - REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.

**HORIZONTAL CURVE NO. 1  
DATA**

PC STA = 3+59.07
PI STA = 5+11.41
PT STA = 6+63.03
R = 1800.00 FT
L = 303.95 FT
T = 152.34 FT
E = 6.44 FT
Δ = 9.675° LT





**COGAN AVENUE  
RECONSTRUCTION**  
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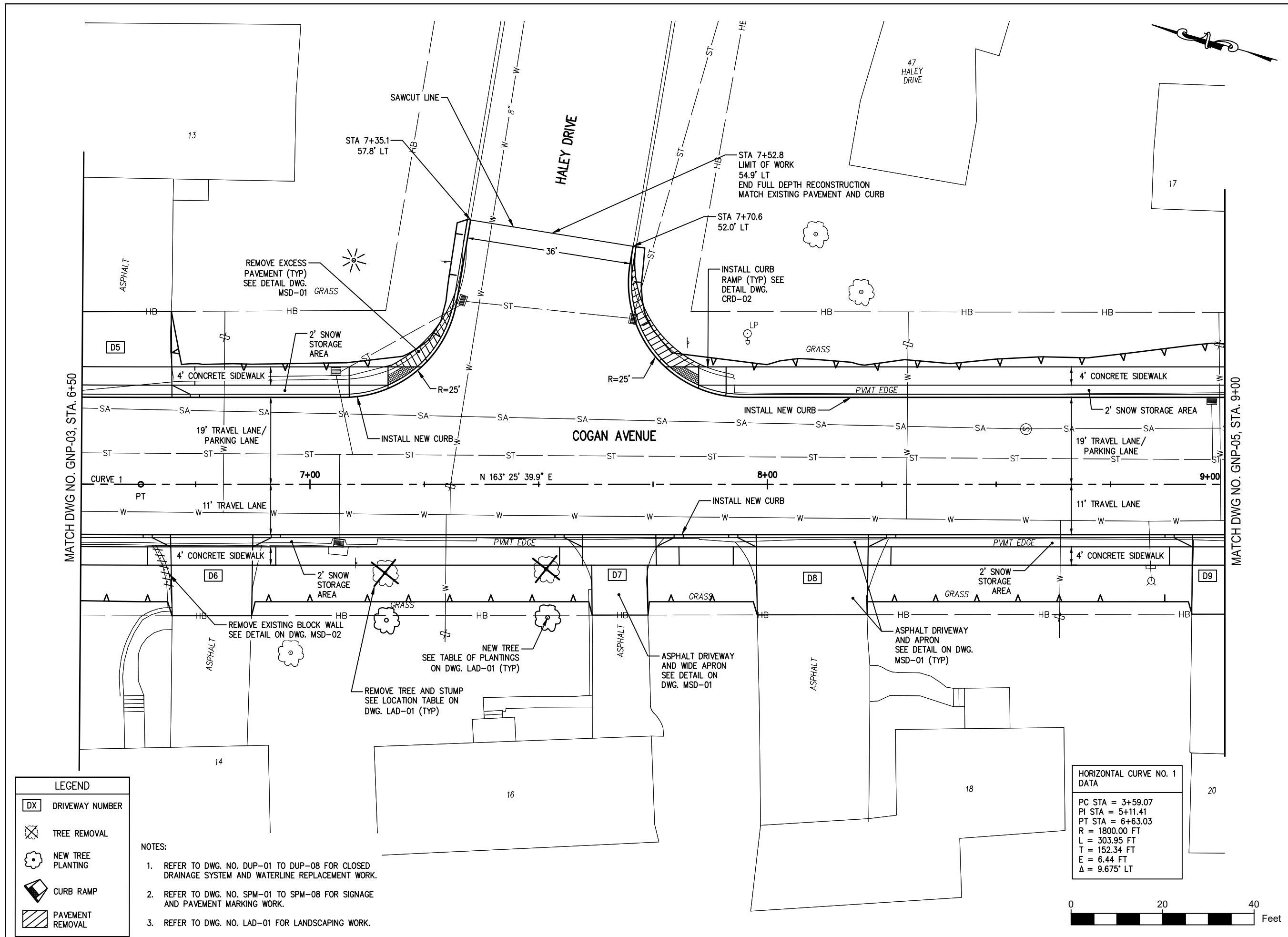
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MARK	DATE	DESCRIPTION
REVISIONS		
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		DATE: MARCH 2021
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		CHECKED BY: T.HUMPHREY
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**GENERAL  
PLAN**

**GNP-04**

SHEET 22 OF 59



MATCH DWG NO. GNP-03, STA. 6+50

MATCH DWG NO. GNP-05, STA. 9+00

**LEGEND**

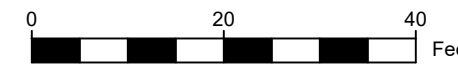
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	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL

**NOTES:**

- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
- REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
- REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.

**HORIZONTAL CURVE NO. 1  
DATA**

PC STA = 3+59.07
PI STA = 5+11.41
PT STA = 6+63.03
R = 1800.00 FT
L = 303.95 FT
T = 152.34 FT
E = 6.44 FT
Δ = 9.675° LT





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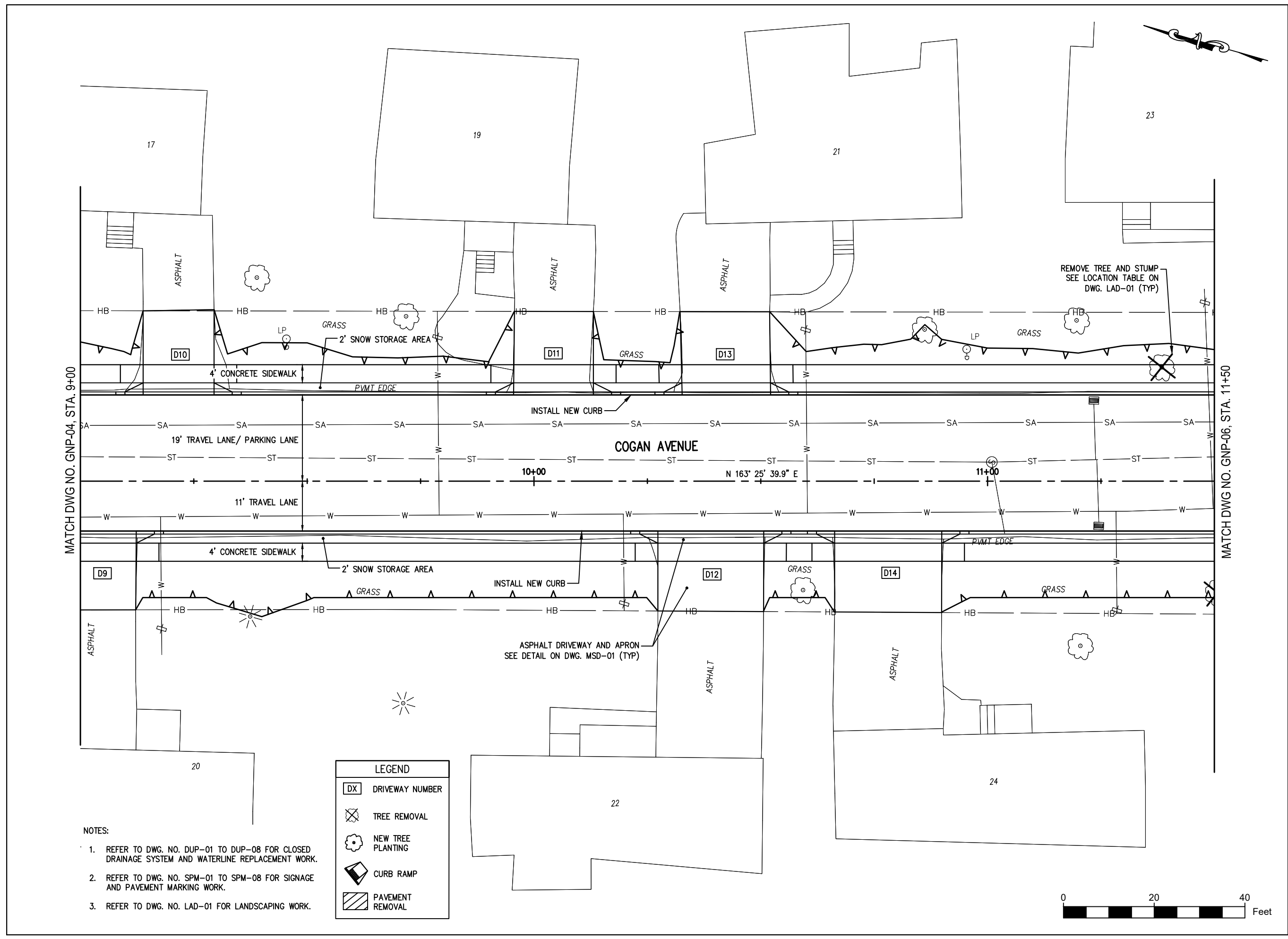
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REVISIONS		
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DESIGNED BY: K. MCARDELL		
CHECKED BY: T. HUMPHREY		
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**GENERAL  
 PLAN**

**GNP-05**

SHEET 23 OF 59

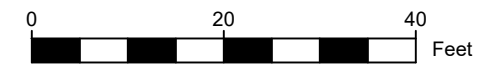
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**LEGEND**

	DRIVEWAY NUMBER
	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL

- NOTES:**
- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
  - REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
  - REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.





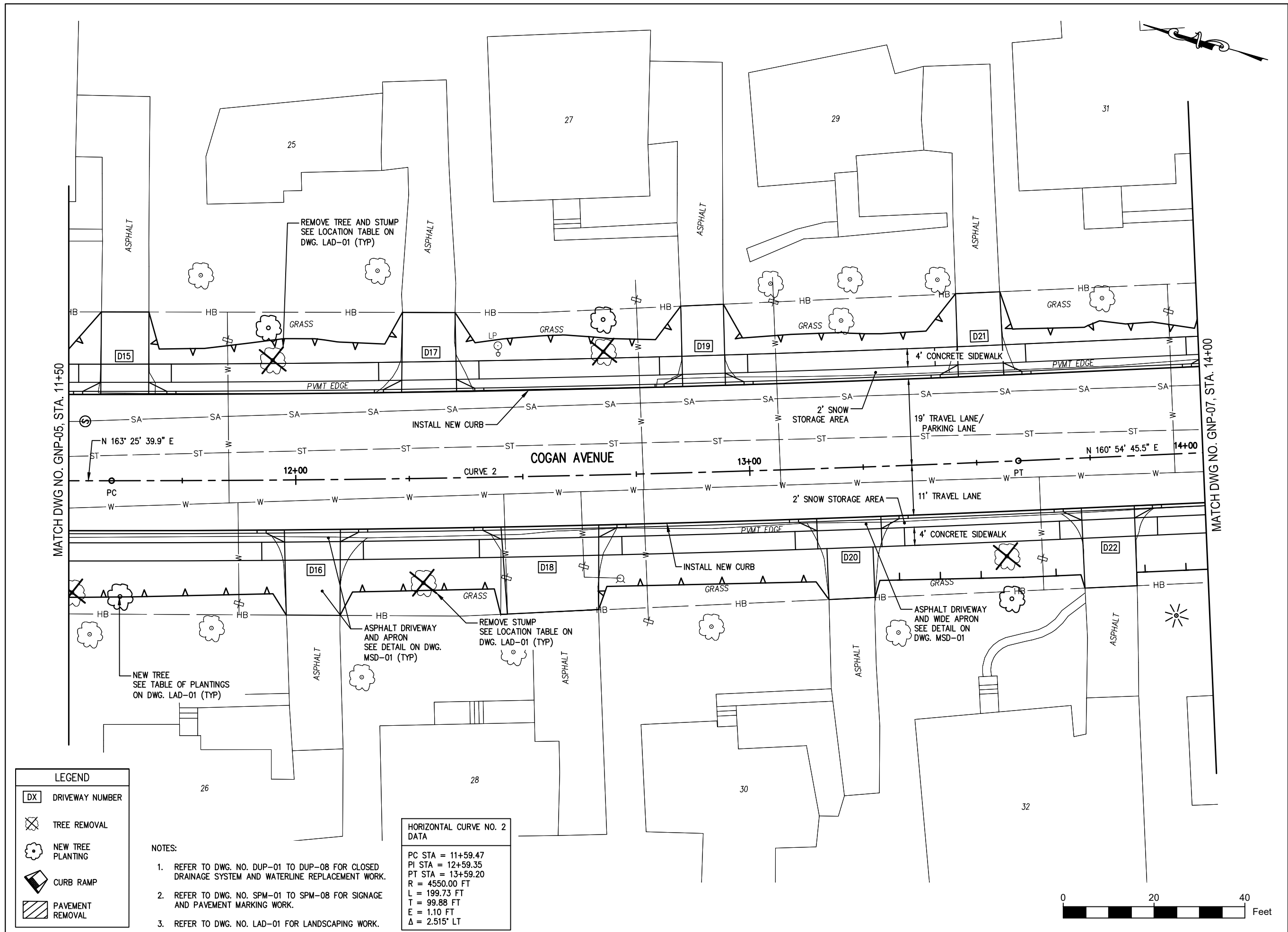
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**GENERAL  
PLAN**

**GNP-06**

SHEET 24 OF 59



MATCH DWG NO. GNP-05, STA. 11+50

MATCH DWG NO. GNP-07, STA. 14+00

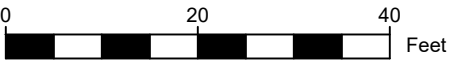
**LEGEND**

	DRIVEWAY NUMBER
	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL

- NOTES:**
- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
  - REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
  - REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.

**HORIZONTAL CURVE NO. 2  
DATA**

PC STA = 11+59.47
PI STA = 12+59.35
PT STA = 13+59.20
R = 4550.00 FT
L = 199.73 FT
T = 99.88 FT
E = 1.10 FT
Δ = 2.515° LT



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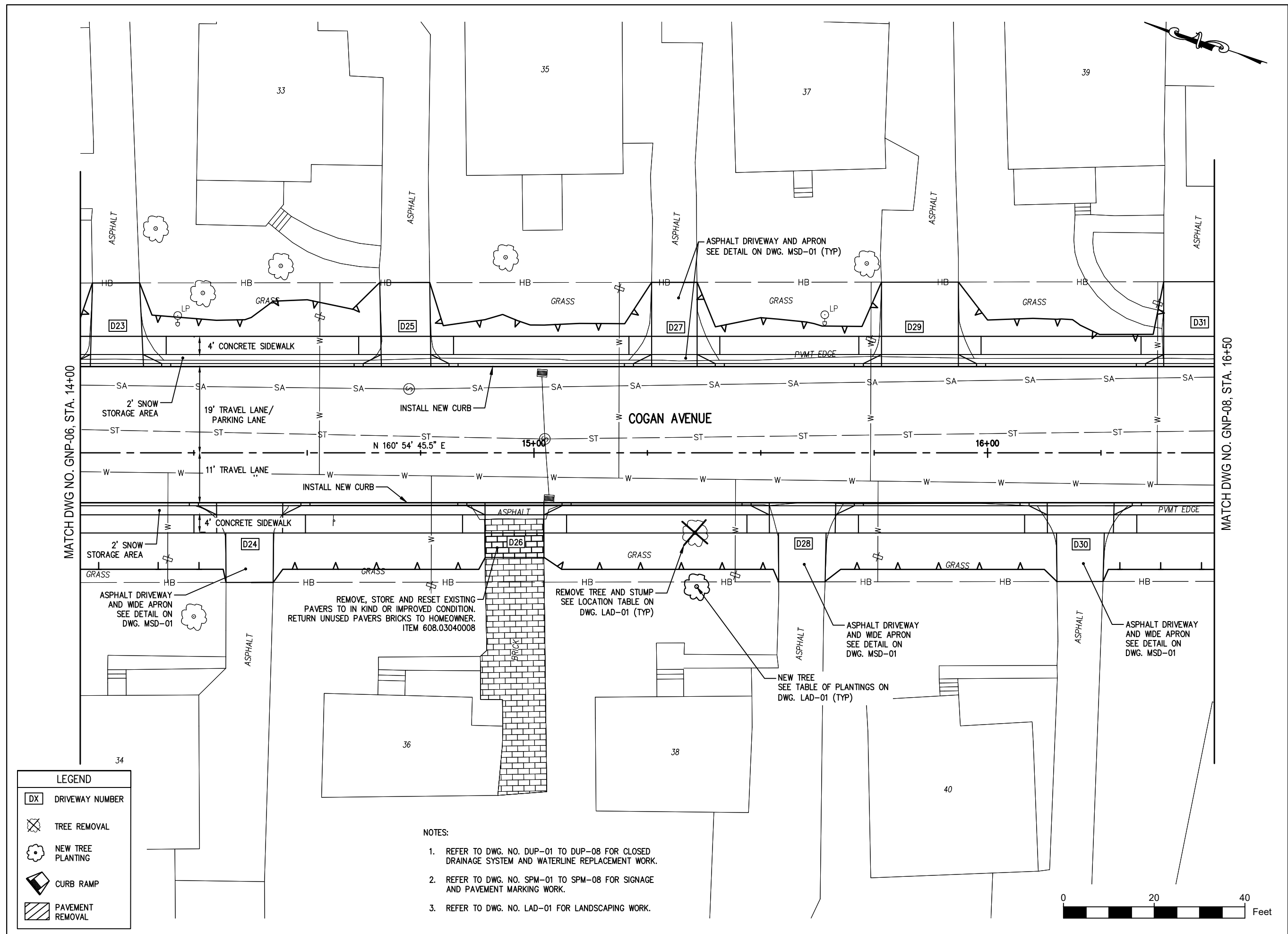
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 CITY OF PLATTSBURGH

MARK	DATE	DESCRIPTION
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PROJECT NO: A54.003.001		
DATE: MARCH 2021		
DRAWN BY: S. GALLAGHER		
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**GENERAL  
 PLAN**

**GNP-07**  
 SHEET 25 OF 59

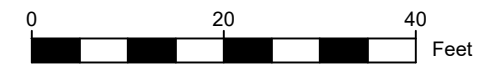
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**LEGEND**

	DRIVEWAY NUMBER
	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL

- NOTES:**
- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
  - REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
  - REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.





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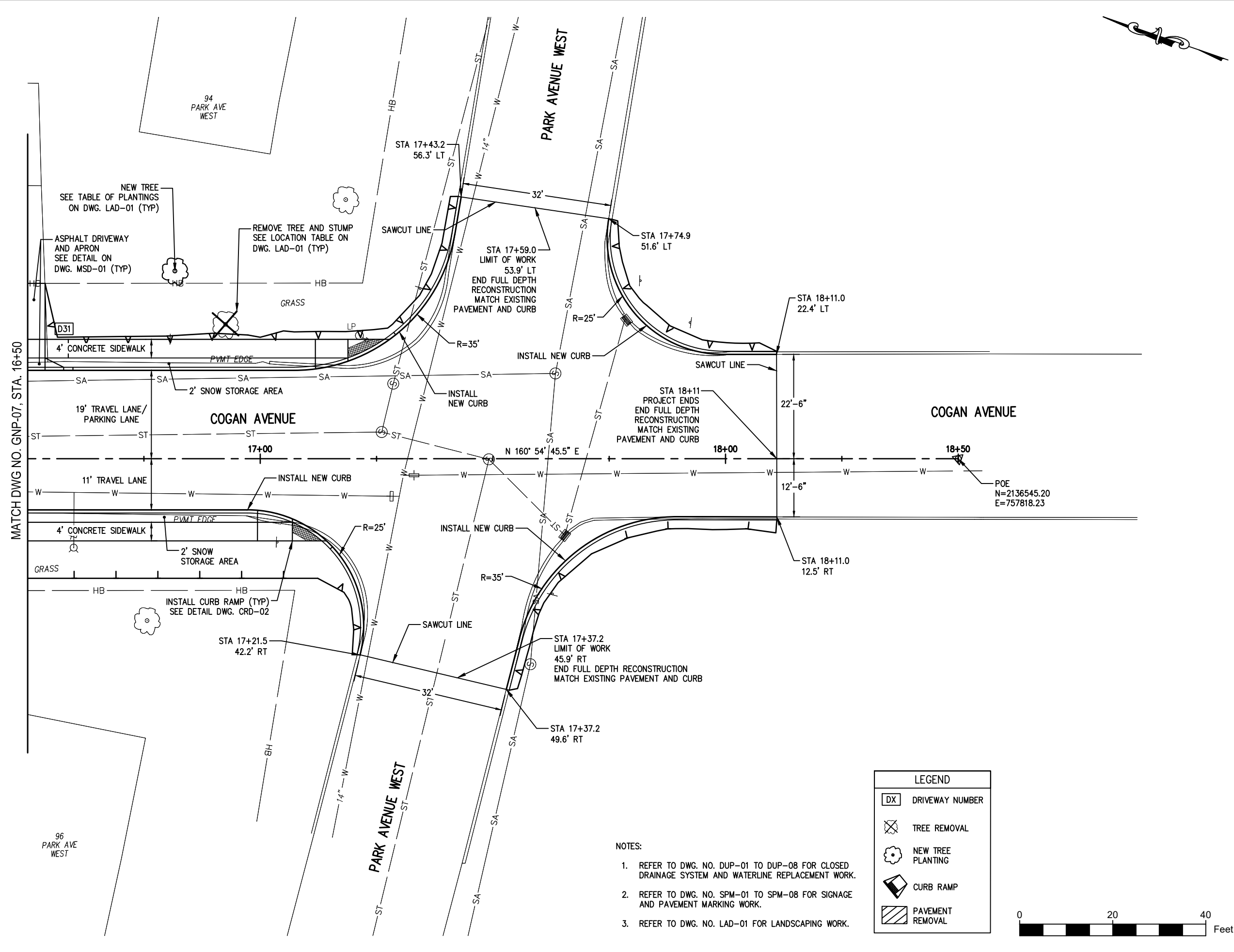
**COGAN AVENUE  
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MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.003.001		
DATE: MARCH 2021		
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DESIGNED BY: K. MCARDELL		
CHECKED BY: T. HUMPHREY		
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**GENERAL  
 PLAN**

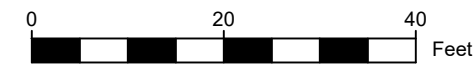
**GNP-08**  
 SHEET 26 OF 59

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- NOTES:
- REFER TO DWG. NO. DUP-01 TO DUP-08 FOR CLOSED DRAINAGE SYSTEM AND WATERLINE REPLACEMENT WORK.
  - REFER TO DWG. NO. SPM-01 TO SPM-08 FOR SIGNAGE AND PAVEMENT MARKING WORK.
  - REFER TO DWG. NO. LAD-01 FOR LANDSCAPING WORK.

LEGEND	
	DRIVEWAY NUMBER
	TREE REMOVAL
	NEW TREE PLANTING
	CURB RAMP
	PAVEMENT REMOVAL





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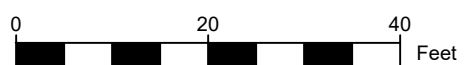
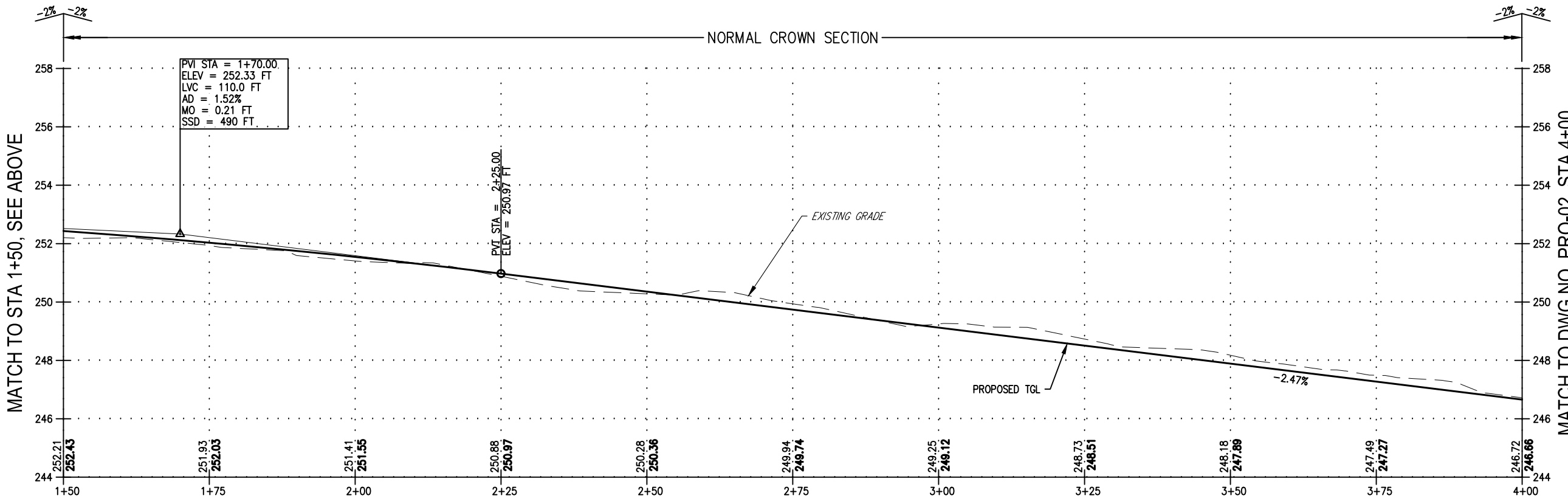
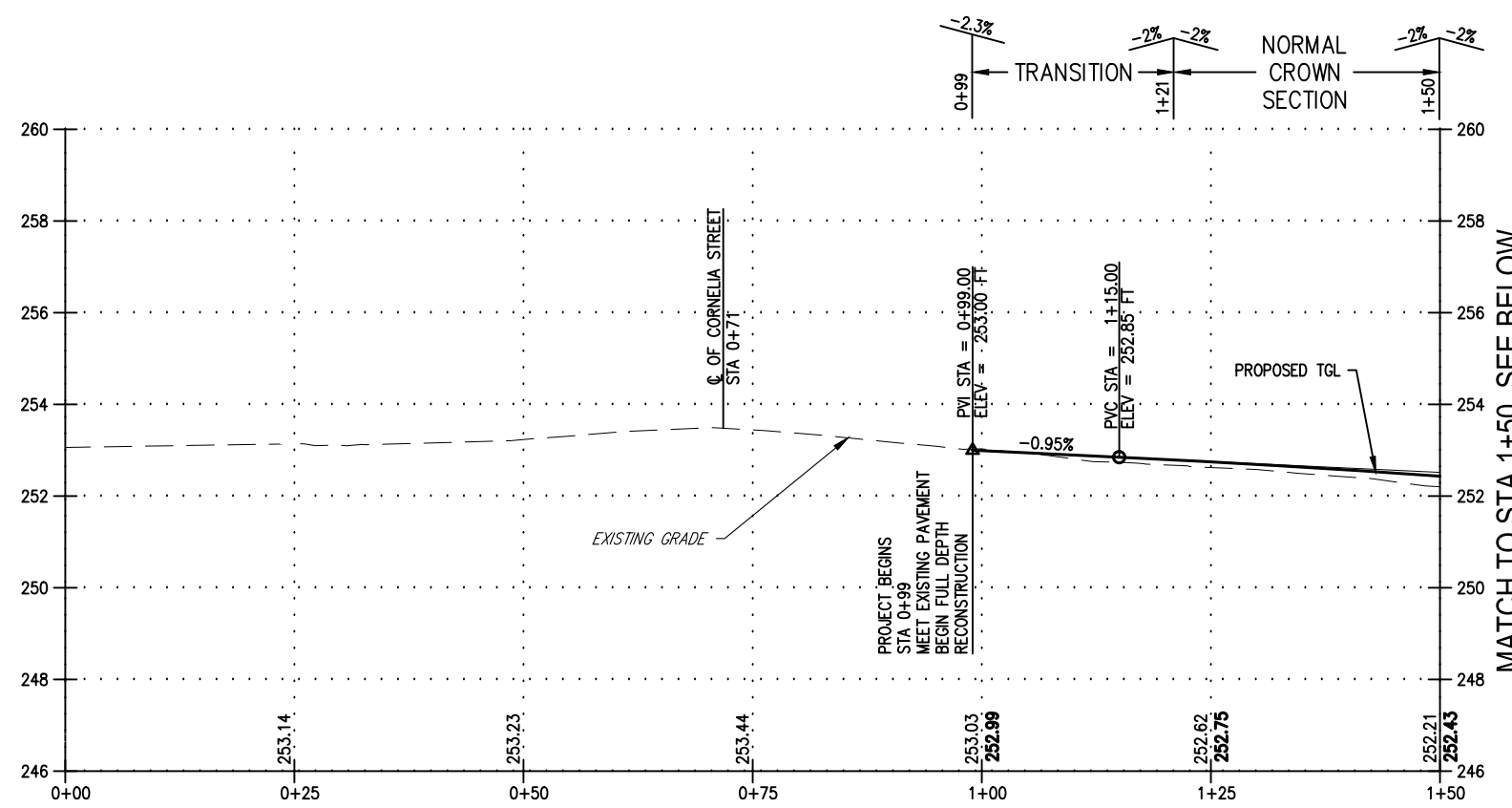
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REVISIONS		
PROJECT NO: A54.003.001		
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**PROFILE**

**PRO-01**

SHEET 27 OF 59



MATCH TO STA 1+50, SEE ABOVE

MATCH TO DWG NO. PRO-02, STA 4+00

MATCH TO STA 1+50, SEE BELOW



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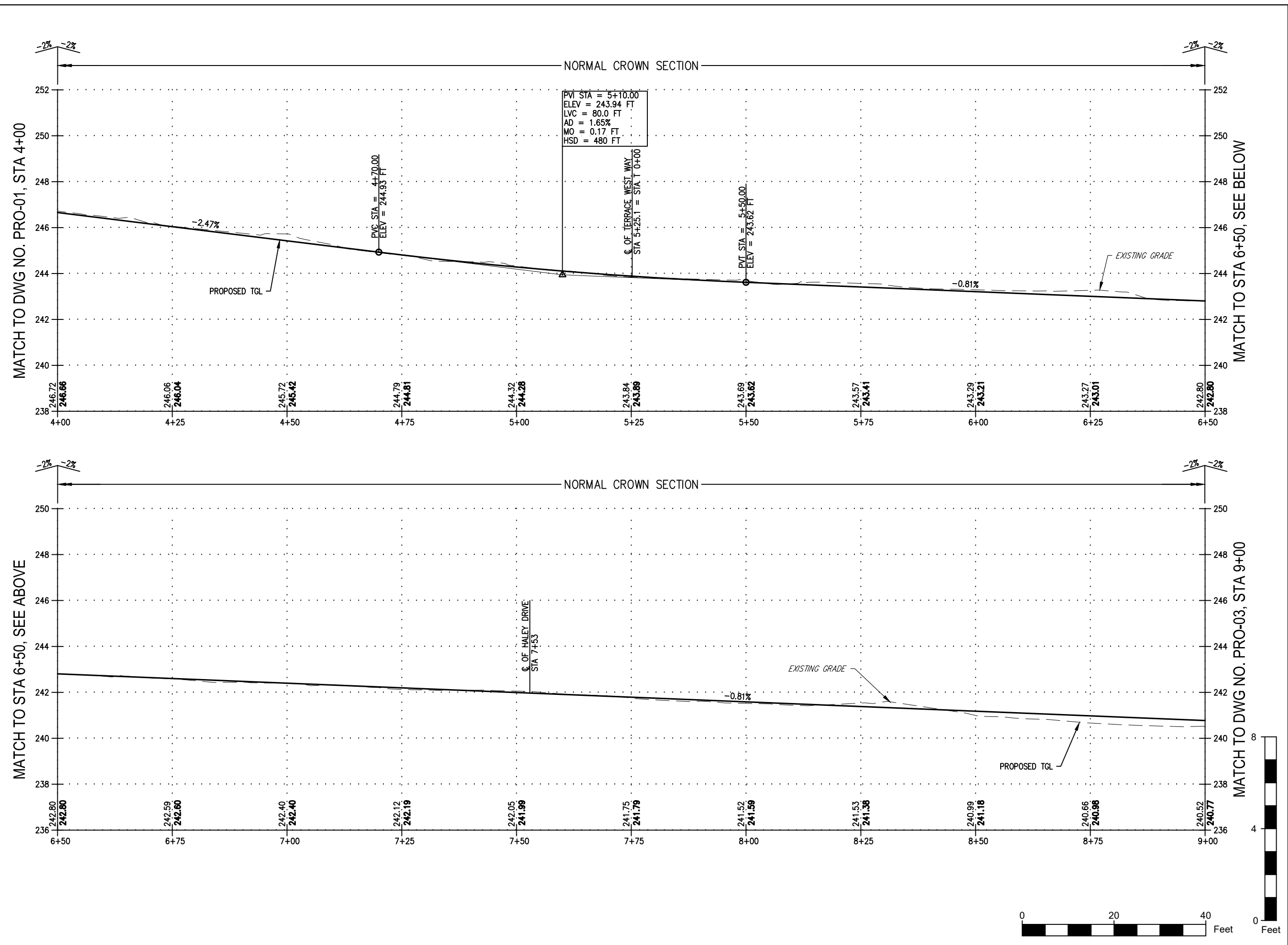
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**PROFILE**

**PRO-02**  
SHEET 28 OF 59





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CITY OF PLATTSBURGH

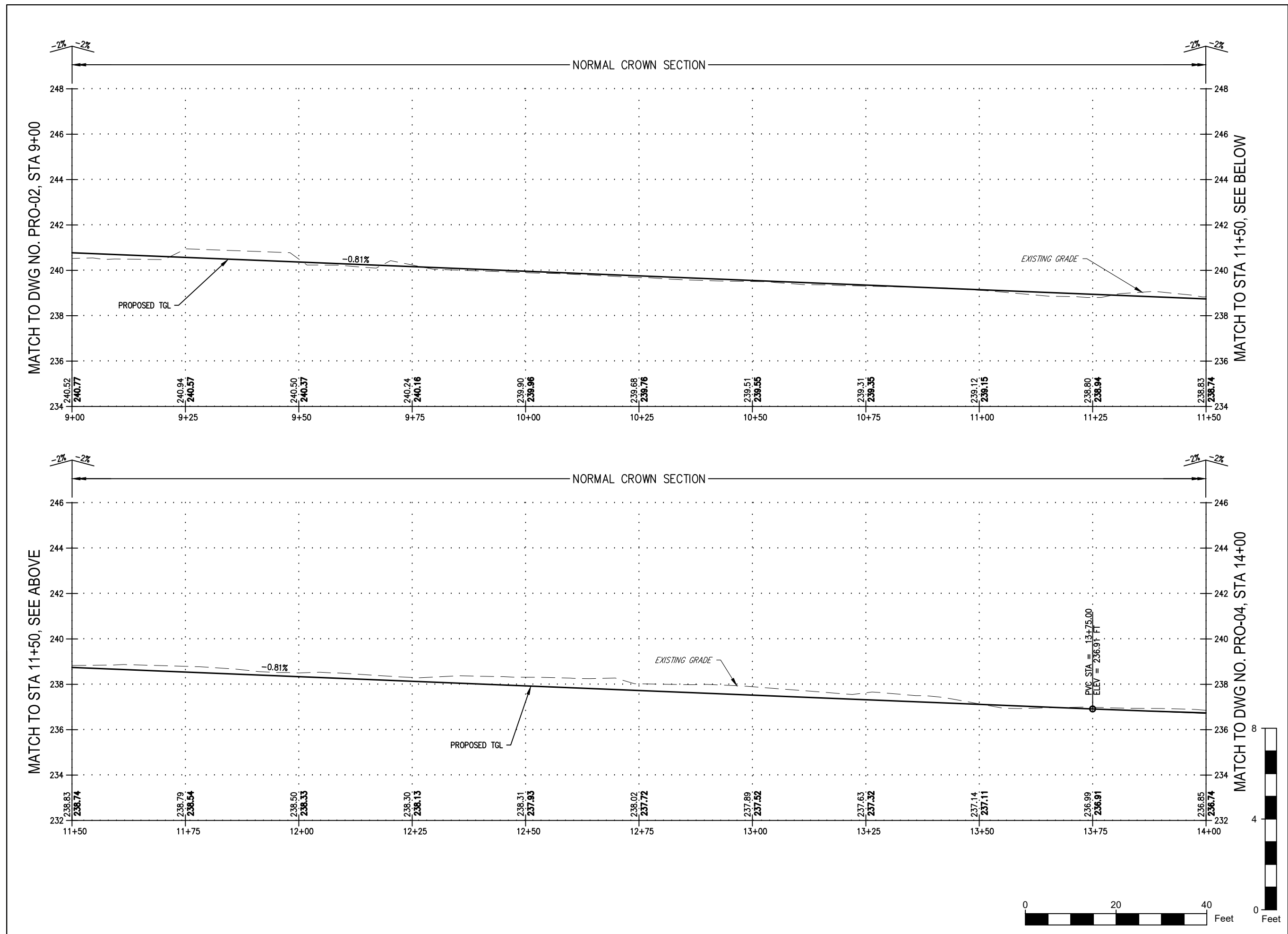
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REVISIONS		
PROJECT NO: A54.003.001		
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DESIGNED BY: K. MCARDELL		
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**PROFILE**

**PRO-03**

SHEET 29 OF 59





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REVISIONS		
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**PROFILE**

**PRO-04**

SHEET 30 OF 59

