

SITE OF WORK



CITY OF PLATTSBURGH
DEPARTMENT OF PUBLIC WORKS
CLINTON COUNTY, NEW YORK

LOCAL PEDESTRIAN SAFETY ACTION PLAN

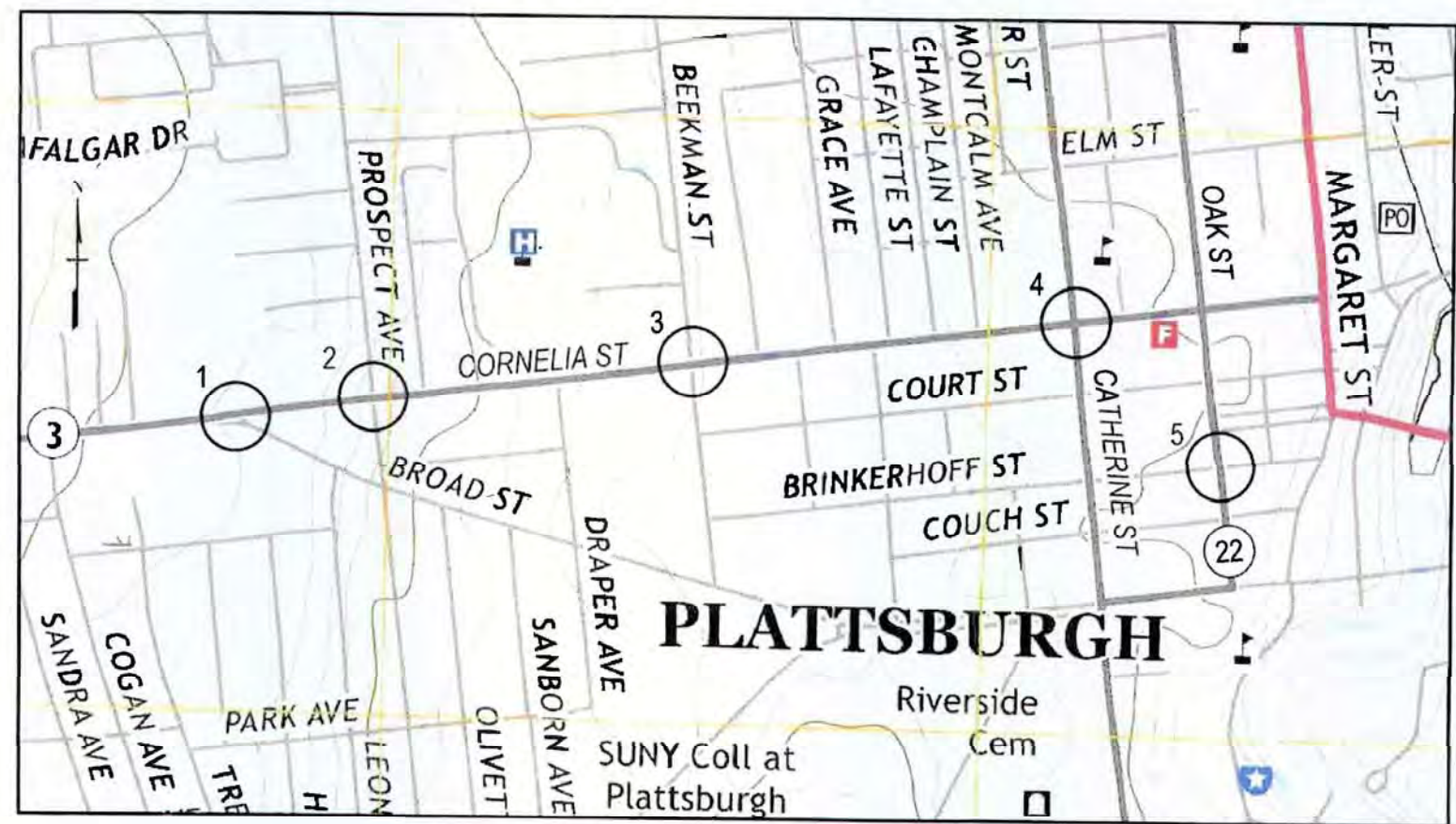
DECEMBER 2020

PROJECT LOCATION

THE PROJECT IS LOCATED AT THE FOLLOWING FIVE INTERSECTIONS IN THE CITY OF PLATTSBURGH:

1. CORNELIA STREET (NYS ROUTE 3) AT BROAD STREET
2. CORNELIA STREET (NYS ROUTE 3) AT PROSPECT AVENUE
3. CORNELIA STREET (NYS ROUTE 3) AT BEEKMAN STREET
4. CORNELIA STREET (NYS ROUTE 3) AT NORTH CATHERINE STREET
5. OAK STREET (NYS ROUTE 22) AT BRINKERHOFF STREET

CONTRACTOR'S NAME _____	
AWARD DATE _____	
COMPLETION DATE _____	
FINAL ACCEPTANCE DATE _____	
CITY ENGINEER _____	
RESIDENT ENGINEER _____	
FINAL COST TOTAL _____	
FISCAL SHARE	COST(S)
1	
2	
3	



PROJECT LOCATION MAP



PREPARED AND RECOMMENDED BY
Todd E. Humphrey 2-1-21

C&S ENGINEERS, INC.
 21 Arkansas Street
 Plattsburgh, NY 12601
 Phone: 315-455-2000
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APPROVED BY: CITY OF PLATTSBURGH

Christopher Rosenquest
 CHRISTOPHER ROSENQUEST
 MAYOR
 DATE 02/14/2021

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL."

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYSDOT POLICIES AND GUIDE LINES AND THE FINAL DESIGN REPORT APPROVED ON 2/27/2020.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

MAINTENANCE RESPONSIBILITY

THE CITY OF PLATTSBURGH WILL BE RESPONSIBLE FOR MAINTENANCE OF THE ROADWAY, SIDEWALKS, AND TRAFFIC SIGNALS CONSTRUCTED UNDER THIS CONTRACT

NYSDOT STANDARD SHEETS

608-01, 609-03, 619-02, 619-04, 619-10, 619-11, 619-12, 619-51, 619-61, 645-01, 645-03, 645-14, 680-01, 680-02, 680-04, 680-05, 680-06, 680-10, 680-13, 685-01

LOCAL PEDESTRIAN SAFETY ACTION PLAN		
FIVE SIGNALIZED INTERSECTIONS		
CITY OF PLATTSBURGH		
COUNTY: CLINTON		
FED. ROAD REG NO	STATE	SHEET NO.
1	N.Y.	1
CAPITAL PROJECT IDENTIFICATION NO	70PS.03	
INDEX ON SHEET NO	2	

Dec 21, 2020 - 12:3pm
F:\Project\A64 - City of Plattsburgh\A64001001 - Local PSDP Five Intersections\Design\CD\Sheet Files\70PS03_ila_01.dwg

FEATURE	SYMBOL
ALIGNMENT	
CONTROL (CENTERLINE)	
CENTER OF CURVATURE	
POINT OF CURVATURE	
POINT OF INTERSECTION	
POINT OF BEGINNING	
POINT OF CURVATURE	
POINT OF END	
POINT ON TANGENT	
POINT OF VERTICAL CURVATURE	
VERTICAL EVENT POINT	
VERTICAL HIGH POINT	
VERTICAL LOW POINT	
CONTROL	
BASELINE	
BASELINE, PROJECTION	
BASELINE, POINT	
BASELINE, POINT ON LINE	
BASELINE, SPUR POINT	
BASELINE, TIE POINT	
BENCHMARK	
POINT, SURVEY MARKER, PERM.	
DRAINAGE	
CATCH BASIN	
FIRE HYDRANT	
CATCH BASIN ROUND	
STORM LINE	
LANDSCAPE	
BOULDER	
DECIDUOUS TREE	
DECIDUOUS SHRUB	
BUILDING	
HEDGE ROW	
PLANTER BED	
PROPERTY LINE	
WOOD LINE	
CHAINLINK FENCE	
STOCKAID FENCE	
RAILING	
ROW MAPPING	
HIGHWAY BOUNDARY	
HIGHWAY BNDRY., APPROX.	
HIGHWAY BNDRY, PT.	
PT., FENCE LOCATION	
PT., IRON PIPE	
PT., IRON ROD	
PT., MONUMENT	
PT., NAIL	
PT., RAILROAD SPIKE	
PT., SPIKE	

FEATURE	SYMBOL
ROW MAPPING	
PT., STAKE	
PT., TREE W/ WIRE	
PT., WALL LOCATION	
SIGNS	
SIGN	
MULTI POST SIGN	
POST	
TRAFFIC CONTROL	
ADA CROSSING PAD	
PULL BOX	
CONTROLLER	
PUSH BUTTON	
SIGN	
POLE	
PEDESTRIAN HEAD	
CONDUIT	
UTILITIES	
SANITARY SEWER MANHOLE	
UNKNOWN MANHOLE	
COMMUNICATION MANHOLE	
WATER VALVE	
GAS FILLCAP	
UNKNOWN VALVE	
MONITORING WELL	
ELECTRIC BOX	
ELECTRIC METER	
UTILITY POLE W/ LIGHT	
UTILITY POLE	
UTILITY POLE DEAD	
GUY WIRE	
PEDESTRIAN LIGHT POLE	
UTILITY LIGHT POLE	
UTILITY MARKOUT	
STORM MANHOLE	
OVERHEAD UTILITY LINE	
UNIDENTIFIED UTILITY LINE	
SANITARY LINE	
WATER LINE	
GAS LINE	
FIBER OPTIC LINE	
ROADWAY	
DROP CURB	

ALIGNMENT	
ABBR.	DESCRIPTION
AZ	AZIMUTH
B	BASELINE
C	CENTERLINE
R	RADIUS
UTILITIES	
ABBR.	DESCRIPTION
E	ELECTRIC
EMH	ELECTRIC MANHOLE
FO	FIBER OPTIC
G	GAS
GP	GUY POLE
GV	GAS VALVE (MAIN LINE)
HYD	HYDRANT
LP	LIGHT POLE
SA	SANITARY SEWER
SMH	SANITARY MANHOLE
ST	STORM SEWER
T	TELEPHONE
W	WATER
WV	WATER VALVE (MAIN LINE)
TOPOGRAPHY (MISCELLANEOUS)	
ABBR.	DESCRIPTION
AOBE	AS ORDERED BY ENGINEER
ASPH	ASPHALT
BLDG	BUILDING
BM	BENCH MARK
CONC	CONCRETE
EIC	ENGINEER IN CHARGE
EP	EDGE OF PAVEMENT
PED POLE	PEDESTRIAN POLE
ROW	RIGHT OF WAY
ST	STREET
SW	SIDEWALK
TYP	TYPICAL
TOPOGRAPHY (DRAINAGE)	
ABBR.	DESCRIPTION
BC	BOTTOM OF CURB
CB	CATCH BASIN
DIA	DIAMETER
ELEV	ELEVATION
MH	MANHOLE
TC	TOP OF CURB

STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
'	ft	LINEAR FEET
ftq	SF	SQUARE FEET
YDr	CY	CUBIC YARD
mi	MI	MILES
AC	AC	ACRES
lb	LB	POUND
TON	TON	TON
GAL	GAL	GALLON

INDEX		
SHEET NO.	DRAWING NO.	TITLE
1	-	TITLE SHEET
2	ILA-1	INDEX, LEGEND, AND ABBREVIATIONS
3	GN-1	GENERAL NOTES
4	EQ-1	ESTIMATE OF QUANTITIES
5	BLT-1	BASELINE TIES & BENCHMARKS
6 TO 8	WZTC-1 TO WZTC-3	WORK ZONE TRAFFIC CONTROL
9	MD-1	MISCELLANEOUS DETAILS
10 TO 14	TSP-1 TO TSP-5	TRAFFIC SIGNAL PLANS
15	TSN-1	TRAFFIC SIGNAL NOTES AND QUANTITIES
16	TSN-2	TRAFFIC SIGNAL DETAILS
17	TSN-3	TRAFFIC SIGNAL QUANTITIES
18 TO 20	CRD-1 TO CRD-3	CURB RAMP DETAILS



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LOCAL PEDESTRIAN SAFETY
ACTION PLAN

CITY OF PLATTSBURGH
CLINTON COUNTY, NEW YORK

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.001.001		
DATE: DECEMBER 2020		
DRAWN BY: MNG		
DESIGNED BY: KAN		
CHECKED BY: TEH		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

INDEX, LEGEND &
ABBREVIATIONS

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GENERAL NOTES

- CONSTRUCTION AND MATERIALS SPECIFICATIONS: STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS, NEW YORK STATE DEPARTMENT OF TRANSPORTATION, OFFICE OF ENGINEERING, DATED JANUARY 1, 2021 WITH CURRENT ADDITIONS AND MODIFICATIONS, EXCEPT AS MODIFIED BY THE "CONSTRUCTION CONTRACT DOCUMENTS".
- THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE CITY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE CITY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY ALL EXISTING CONDITIONS AND DIMENSIONS PRIOR TO THE START OF WORK AND/OR PREPARATION OF SHOP DRAWINGS. THE CONTRACTOR SHALL REPORT ANY DISCREPANCIES TO THE ENGINEER IMMEDIATELY AND PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.
- WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THOSE ITEMS.
- THE COST OF ALL JOINT MATERIAL SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT, UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- ALL DISTURBED AREAS SHALL BE RESTORED BY THE TREATMENT SHOWN ON THE PLANS, BY TOPSOIL AND SEEDING, OR AS ORDERED BY ENGINEER. GRADING SHALL BE SHAPED TO ALLOW SURFACE DRAINAGE TO RETURN TO ITS ORIGINAL CONDITION.
- TYPICAL WORK LIMITS ARE SHOWN ON THE PLANS. ACTUAL LIMITS OF WORK SHALL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION. THE ENGINEER SHALL DEFINE SPECIFIC LIMITS WHERE QUESTIONS ARISE.
- THE ROADWAY SHALL BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES.
- MATERIALS, EQUIPMENT AND VEHICLES SHALL NOT BE STORED OR PARKED WITHIN THE HIGHWAY RIGHT-OF-WAY BEFORE WORK BEGINS OR AFTER CONTRACTOR'S OPERATIONS ARE SHUT DOWN. STAGING AREAS OUTSIDE THE RIGHT-OF-WAY SHALL BE USED TO STOCKPILE ALL CONSTRUCTION MATERIALS, DURING WORKING HOURS, NO CONSTRUCTION MATERIAL MAY BE STORED OR PLACED ON THE ROADWAY OR ROADBED EXCEPT WITHIN A PROTECTED WORK AREA.

DEFINITIONS

- THE WORDS "SHALL", "SHOULD", AND "MAY", AS USED IN THE CONTRACT DOCUMENTS, HAVE THE FOLLOWING MEANINGS:

SHALL - A MANDATORY CONDITION. IN THE DESIGN, APPLICATION, OR LOCATION OF DEVICES REQUIREMENTS HAVING "SHALL" STIPULATIONS ARE MANDATORY. NO DISCRETION IN FOLLOWING THEM IS ALLOWED.

SHOULD - AN ADVISORY CONDITION. WHERE "SHOULD" IS USED IN RELATION TO A PROVISION, THAT PROVISION IS RECOMMENDED, AND NORMALLY IS TO BE FOLLOWED, BUT IS NOT MANDATORY, DEVIATION FROM SUCH PROVISIONS IS PERMISSIBLE IF, AND TO THE EXTENT, THERE IS JUSTIFIABLE CAUSE TO DO SO.

MAY - A PERMISSIVE CONDITION. NO REQUIREMENT FOR DESIGN OR APPLICATION IS INTENDED.

HIGHWAY WORK PERMITS

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED STATE, COUNTY, AND MUNICIPAL PERMITS WITHIN HIGHWAY RIGHTS-OF-WAY.

HOURS OF OPERATION

- WORK HOURS SHALL BE LIMITED TO BETWEEN 7 AM AND 7 PM. NO NIGHT WORK IS ALLOWED UNDER THIS CONTRACT. CONTRACTOR SHALL ABIDE BY ALL LOCAL NOISE ORDINANCES.

SUBMISSION GUIDELINES

- ALL SHOP DRAWINGS, DETAILS AND COMPUTATION SHEETS SUBMITTED FOR THIS CONTRACT SHALL BE IN U.S CUSTOMARY UNITS.
- THE CONTRACTOR SHALL SUBMIT ALL SHOP DRAWINGS, DETAILS AND COMPUTATION SHEETS TO THE ENGINEER FOR APPROVAL. THE ENGINEER SHALL BE PERMITTED THE LONGEST OF THE FOLLOWING TIME LIMITS TO RENDER A DECISION ON THE CONTRACTOR' SUBMISSION:

A. TEN (10) WORKING DAYS

B. TWO (2) WORKING DAYS FOR EACH DRAWING OF A SET OF DRAWINGS OR

C. ONE (1) WORKING DAY FOR EVERY FOUR (4) COMPUTATION SHEETS. ANY SHEET WRITTEN ON BOTH SIDES WILL BE CONSIDERED TWO (2) COMPUTATION SHEETS.
- ALL THE TIME LIMITS WILL BE MEASURED BASED UPON THE RECEIPT OF ALL REQUIRED INFORMATION BY THE ENGINEER.
- UPON APPROVAL, ONE (1) SET OF DRAWINGS AND COMPUTATIONS SHALL BE FURNISHED BY THE CONTRACTOR TO THE ENGINEER FOR THEIR RECORDS.

STAGING AREAS

- THERE ARE NO AREAS WITHIN THE CONTRACT LIMITS AVAILABLE FOR STAGING EQUIPMENT OR OPERATIONS.

SURVEY CONTROL

- CONTRACTOR IS RESPONSIBLE FOR ALL SURVEY NEEDED TO COMPLETE THE WORK. PAYMENT FOR SURVEY WORK WILL BE MADE UNDER ITEM 625.01-SURVEY OPERATIONS.
- ALL RIGHT-OF-WAY MONUMENTS AND PROPERTY CORNERS ARE TO BE PROTECTED, SAFEGUARDED AND PRESERVED. ANY RIGHT-OF-WAY OR PROPERTY CORNERS THAT ARE DISTURBED BY CONTRACTOR'S OPERATIONS SHALL BE REPLACED BY A NYS LICENSED SURVEYOR AT THE CONTRACTOR'S EXPENSE.

EXISTING DRAINAGE FACILITIES

- THE CONTRACTOR SHALL BECOME ACQUAINTED WITH THE DRAINAGE CHARACTERISTICS OF THE AREA IN ORDER TO PROGRESS WORK EFFICIENTLY WITH FULL KNOWLEDGE OF THE POTENTIAL DRAINAGE PROBLEMS.
- THE CONTRACTOR SHALL ENSURE ALL EXISTING DRAINAGE SYSTEMS, INCLUDING CATCH BASINS AND CULVERTS, ARE KEPT CLEAN AND FREE FLOWING FOR THE DURATION OF THE CONTRACT, AT NO COST TO THE CITY (UNLESS OTHERWISE SPECIFIED).

UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING AND MAINTAINING SERVICE OF ALL UTILITIES ENCOUNTERED IN THE WORK AREA. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ALL DISRUPTIONS TO UTILITY SERVICE CAUSED BY THE CONTRACTOR'S OPERATIONS. DAMAGE TO EXISTING UTILITIES AND APPURTENANCES RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED, AT THE CONTRACTOR'S SOLE EXPENSE, TO THE OWNER'S SATISFACTION OR AS ORDERED BY THE ENGINEER.
- IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE, CAUSING AN INTERRUPTION IN SAID SERVICE, THE CONTRACTOR SHALL IMMEDIATELY BEGIN WORK TO RESTORE SERVICE AND SHALL NOT CEASE SUCH WORK UNTIL SERVICE IS RESTORED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES, INCLUDING STREET LIGHTING CONDUITS, TRAFFIC SIGNAL CONDUITS ETC. PRIOR TO DRIVING SIGN POSTS OR EXCAVATING WITHIN THE LIMITS OF THE CONTRACT. THE COST OF REPAIRING DAMAGE TO CONDUITS, CONDUCTORS AND APPURTENANCES SHALL BE PAID FOR BY THE CONTRACTOR.
- THE APPROXIMATE LOCATION OF THE UNDERGROUND UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL VERIFY THE TRUE LOCATION BEFORE COMMENCING WORK. THE UTILITY LOCATION QUALITY LEVEL DEFINITION SHOWN ON THESE PLANS DEFINED AS FOLLOWS:

QUALITY LEVEL C - RECORD INFORMATION PROVIDED BY UTILITY OWNERS WAS PLOTTED ON THE CONTRACT PLANS. DEPTHS WERE NOT FIELD VERIFIED. PHYSICAL SURFACE FEATURES LIKE MANHOLES, VALVE BOXES AND HYDRANTS HAVE BEEN FIELD LOCATED.

SIGNS

- ALL SIGNS SHALL BE MAINTAINED BY THE CONTRACTOR AND AS DIRECTED BY THE ENGINEER.
- IF EXISTING SIGNS ARE DAMAGED AS A RESULT OF THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR SHALL SUPPLY A NEW SIGN, MEETING THE SAME STANDARD, AT NO COST TO THE OWNER.

RIGHT OF WAY

- ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY (ROW) IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR IS TO ASSURE HIMSELF THAT ALL WORK IS BEING PERFORMED WITHIN THE ROW, INCLUDING BUT NOT LIMITED TO VEHICLE ACCESS; STORAGE OF EQUIPMENT, MATERIALS, DEBRIS AND WASTE; LANDSCAPING; VEGETATION REMOVAL AND MANAGEMENT; GRADING, SEEDING AND THE INSTALLATION OF TURF; AND THE INSTALLATION OF ANY FENCES OR PROTECTIVE BARRIER.
- IF CONTRACTOR IS UNABLE TO IDENTIFY THE LIMITS OF THE RIGHTS-OF-WAY WHEN THE CONTRACT CALLS FOR WORK IN THOSE VICINITIES, THE CONTRACTOR MUST CONTACT THE ENGINEER IN CHARGE FOR DEFINITIVE BOUNDARY DETERMINATIONS BEFORE ANY WORK MAY BE INITIATED AT THOSE LOCATIONS (STANDARD SPECIFICATIONS SECTIONS 105-10 AND 625).
- IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS, RELEASES FOR ANY NON-ESSENTIAL CONTRACT WORK OUTSIDE OF THE EXISTING RIGHTS-OF-WAY, INCLUDING PLANTINGS, LANDSCAPING OR DRIVEWAY ENHANCEMENT, WILL BE PROVIDED BY THE PROJECT ENGINEER AND IN NO INSTANCE ARE TO BE SECURED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDINGS OUTSIDE OF THE RIGHTS-OF-WAY FOR ANY REASON WITHOUT FIRST SECURING WRITTEN PERMISSION FROM THE PROPERTY OWNER (STANDARD SPECIFICATIONS SECTION 105-15).
- THE CONTRACTOR WILL BE HELD LIABLE FOR ANY DAMAGES DONE. ANY SUCH INJURIES OR DAMAGES SHALL BE SATISFACTORILY REPAIRED OR ITEMS REPLACED AT THE CONTRACTOR'S EXPENSE (STANDARD SPECIFICATIONS SECTION 107-08).

DISPOSAL OF CONSTRUCTION AND DEMOLITION DEBRIS

- THERE ARE NO AREAS WITHIN THE CONTRACT LIMITS AVAILABLE FOR DISPOSAL OF DEBRIS.

HAZARDOUS WASTE AND CONTAMINATED MATERIALS

- THE CONTRACTOR SHOULD BE AWARE THAT THE PROJECT SITES HAVE BEEN SCREENED AND NYSDEC RECORDS INDICATE A FUEL SPILL AT THE NORTHEAST CORNER OF CORNELIA AND BROAD STREETS (STEWART'S SHOPS). THE INCIDENT IS CLOSED AND NO CONTAMINATED SOIL IS EXPECTED TO BE ENCOUNTERED. IF THE CONTRACTOR ENCOUNTERS ANY SUSPECT SOIL, NOTIFY THE ENGINEER IN CHARGE IMMEDIATELY.

TEMPORARY SOIL EROSION AND SEDIMENT CONTROL NOTES

- THROUGHOUT THE DURATION OF THE CONTRACT, CONTRACTOR SHALL PROTECT ALL WATER COURSES FROM WATER BORNE SEDIMENT OR POLLUTANTS ORIGINATING FROM ANY WORK DONE IN SUPPORT OF THIS PROJECT.
- IN ACCORDANCE WITH SECTIONS 107-12 AND 209-3.01 OF THE NYSDOT STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL DESIGNATE AN EROSION AND SEDIMENT CONTROL SUPERVISOR FOR THE PROJECT. THE SUPERVISOR SHALL BE RESPONSIBLE FOR IMPLEMENTING THE EROSION AND SEDIMENT CONTROL MEASURES AND FOR INSPECTING AND MAINTAINING THE CONTROL MEASURES. THE NAME AND QUALIFICATIONS (TRAINING AND EXPERIENCE) OF THIS INDIVIDUAL SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO STARTING EARTHWORK.
- EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY SOIL DISTURBANCE FOR WHICH THEY ARE INTENDED AND SHALL REMAIN IN PLACE UNTIL SOILS ARE PERMANENTLY STABILIZED.
- NO WET OR FRESH CONCRETE, LEACHATE, MATERIAL, OR DEBRIS SHALL BE ALLOWED TO ESCAPE INTO A WATER BODY OR WETLAND, NOR SHALL WASHINGS FROM CONCRETE TRUCKS, MIXERS OR OTHER DEVICES BE ALLOWED TO ENTER WATER BODY OR WETLAND. ANY MATERIAL OR DEBRIS ACCIDENTALLY DROPPED INTO THE CHANNEL SHALL BE IMMEDIATELY AND COMPLETELY REMOVED AND DEPOSITED IN AN UPLAND AREA.
- THE CONTRACTOR SHALL COVER TEMPORARY STOCKPILES OF ERODIBLE MATERIAL (SUCH AS TOPSOIL OR EARTH FILL) WITH POLY SHEETING, OR RING THE STOCKPILES WITH SILT FENCE TO CONTROL EROSION. POLY SHEETING SHALL COMPLETELY COVER THE STOCKPILE AND BE SECURELY ANCHORED AT ALL TIMES. ANY POLY SHEETING OR SILT FENCE THAT IS DAMAGED SHALL BE PROMPTLY REPAIRED OR REPLACED AS DIRECTED BY THE ENGINEER. RINGED STOCKPILES EXPOSED OR EXPECTED TO BE EXPOSED FOR LONGER THAN 7 CALENDAR DAYS SHALL IMMEDIATELY BE STABILIZED WITH APPROPRIATE MEASURES. THE COST OF COVERING AND RINGING/STABILIZING STOCKPILES SHALL BE INCLUDED IN THE PRICE BID FOR THE CORRESPONDING STOCKPILED MATERIAL.

EMERGENCY REPAIRS

- THE CONTRACTOR SHALL PROVIDE THE LOCAL POLICE THE NAME, ADDRESS AND TELEPHONE NUMBER OF A PERSON OR PERSONS AUTHORIZED OUTSIDE THE NORMAL CONTRACT WORKING HOURS TO SECURE AND USE LABOR, MATERIAL AND EQUIPMENT FOR EMERGENCY REPAIRS TO MAKE SAFE THE ENTIRE AREA OF THE CONTRACT. DUPLICATE COPIES OF SUCH NOTICES SHALL BE FILED WITH THE RESIDENT ENGINEER, AND CITY OF PLATTSBURGH.

CONCRETE MANAGEMENT NOTES

- STORE CONCRETE, GROUT, AND MORTAR UNDER COVER AND AWAY FROM DRAINAGE AREAS.
- CONCRETE TRUCKS AND TRANSFER CHUTES MAY BE WASHED OUT ON-SITE IN AN AREA APPROVED BY THE EIC. UTILIZE A CONCRETE WASHOUT TO COLLECT ALL WASH WATER AND CONCRETE WASTE.
- THE WASHOUT AREA SHALL BE LOCATED AWAY FROM STORM DRAINS, OPEN DITCHES OR WATER BODIES.
- UPON COMPLETION OF THE CONCRETE WORK, BREAK UP, REMOVE, AND HAUL AWAY SOLID CONCRETE THAT HAS ACCUMULATED IN THE WASHOUT.
- PREVENT WET OR FRESH CONCRETE OR LEACHATE FROM REACHING WATERS OF NYS. DO NOT WASH CONCRETE TRUCKS, MIXERS, OR ANY OTHER DEVICES AT A LOCATION THAT LEADS TO WATERS OF NYS.

FUEL AND EQUIPMENT STORAGE NOTES

- ALL CONSTRUCTION MATERIALS AND EQUIPMENT SHALL BE STORED IN AN AREA APPROVED BY THE ENGINEER.
- FUEL STORAGE SHALL BE CONDUCTED IN ACCORDANCE WITH APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS.
- ON SITE VEHICLE AND EQUIPMENT FUELING SHOULD ONLY OCCUR IN A DESIGNATED FUEL STORAGE AREA. SUBSTITUTE LOCATIONS MAY BE USED SUBJECT TO APPROVAL BY THE ENGINEER.
- VEHICLE AND EQUIPMENT FUELING (INCLUDING FUELING OF HAND HELD EQUIPMENT) SHALL BE CONDUCTED IN ACCORDANCE WITH THE FOLLOWING:

A. AWAY FROM STORM DRAIN INLETS, DRAINAGE FACILITIES, OR WATERCOURSES.

B. WITHIN A BERMED AREA TO PREVENT RUN-ON, RUNOFF, AND TO CONTAIN SPILLS.

C. STORE PORTABLE FUEL CONTAINERS FOR HAND HELD EQUIPMENT IN A TUB OR EQUIVALENT DEVICE TO AVOID SPILLS AND LEAKS.

D. USE SECONDARY CONTAINMENT TECHNIQUES FOR FUELING OF HAND HELD OR PORTABLE EQUIPMENT, SUCH AS DRAIN PANS OR DROP CLOTHS TO CATCH SPILLS OR LEAKS.

E. SIGNAGE THAT FUEL TANKS SHOULD NOT BE "TOPPED OFF". AN ADEQUATE SUPPLY OF SPILL CLEAN UP MATERIALS SHALL BE READILY ACCESSIBLE TO ALL FUELING ACTIVITIES.

ASBESTOS

- ALL SITES HAVE BEEN SCREENED FOR THE PRESENCE OF ASBESTOS CONTAINING MATERIALS. NONE WERE FOUND BUT MAY STILL EXIST. IF THE CONTRACTOR ENCOUNTERS SUSPECT MATERIALS, NOTIFY THE ENGINEER IN CHARGE IMMEDIATELY. IF NECESSARY, SCREENING, TESTING, AND ABATEMENT WILL BE PERFORMED AND PAID UNDER FORCE ACCOUNT.



C&S
COMPANIES

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LOCAL PEDESTRIAN SAFETY
ACTION PLAN
CITY OF PLATTSBURGH
CLINTON COUNTY, NEW YORK

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.001.001		
DATE: DECEMBER 2020		
DRAWN BY: MNG		
DESIGNED BY: KAN		
CHECKED BY: TEH		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

GENERAL
NOTES

GNN-1

SHEET 3 OF 20

Dec 21, 2020 - 1:24pm
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TABLE OF QUANTITIES								
ITEM	DESCRIPTION	UNIT	BASE BID QUANTITIES		DEDUCT ALTERNATE #1 QUANTITIES		DEDUCT ALTERNATE #2 QUANTITIES	
			PROP.	AS-BUILT	PROP.	AS-BUILT	PROP.	AS-BUILT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	170		48		7	
203.03	EMBANKMENT IN PLACE	CY	10		0		0	
206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	LF	225		60		35	
209.1702	DRAINAGE STRUCTURE INLET PROTECTION, GRAVEL BAG - TEMPORARY	CY	4		1		0	
304.12	SUBBASE COURSE, TYPE 2	CY	92		27		0	
402.000013	PLANT PRODUCTION QUALITY ADJUSTMENT TO HMA ITEMS	QU	2		0		0	
402.098203	9.5 F2 TOP COURSE HMA, 80 SERIES COMPACTION	TON	7		2		0	
402.258903	25 F9 BINDER COURSE HMA, 80 SERIES COMPACTION	TON	10		3		0	
402.378903	37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION	TON	37		10		0	
407.0102	DILUTED TACK COAT	GAL	158		42		0	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	760		200		0	
608.0101	CONCRETE SIDEWALKS AND DRIVEWAYS	CY	43		15		7	
608.020102	HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS, AND BICYCLE PATHS, AND VEGETATION CONTROL STRIPS	TON	4		0		0	
608.21000003	CAST IRON EMBEDDED DETECTABLE WARNING UNITS	SY	36		9		8	
609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF150	LF	265		96		0	
609.0403	CAST-IN-PLACE CONCRETE CURB TYPE M150	LF	147		26		0	
610.16010020	TURF ESTABLISHMENT - PERFORMANCE	SY	50		9		0	
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS	NEC		NEC		NEC	
619.04	TYPE III CONSTRUCTION BARRICADE	EA	68		16		8	
619.1613	MAINTAIN TRAFFIC SIGNAL EQUIPMENT (REQUIREMENT C)	INTM	5		1		1	
625.01	SURVEY OPERATIONS	LS	NEC		NEC		NEC	
627.50140008	CUTTING PAVEMENT	LF	98		0		0	
635.0103	CLEANING AND PREPARATION OF PAVEMENT SURFACES - LINES	LF	7720		2680		890	
645.81	TYPE A SIGN POSTS	EA	2		0		1	
645.85	POLE MOUNTED SIGN SUPPORT SYSTEM (BAND MOUNTED)	EA	2		0		0	
647.31	RELOCATE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE I (UNDER 30 SQUARE FEET)	EA	2		0		1	
647.61	REMOVE AND DISPOSE GROUND MOUNTED TYPE A SIGN SUPPORT(S), FOUNDATIONS, AND ANY ATTACHED SIGNS - SIZE I (UNDER 30 SF)	EA	2		0		1	
680.5001	POLE EXCAVATION AND CONCRETE FOUNDATION	CY	8		2		2	
680.510501	PULLBOX - RECTANGULAR, 26" X 18", REINFORCED CONCRETE	EA	14		5		5	
680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	LF	225		60		35	
680.520108	CONDUIT, METAL STEEL, ZINC COATED, 3 NPS	LF	1255		330		320	
680.53	CONDUIT JACKING OR BORING	LF	1255		330		320	
680.6708	TRAFFIC SIGNAL POLE POST TOP MOUNT, 8 FEET MOUNTING HEIGHT	EA	20		3		4	
680.700603	RISER ASSEMBLY, 1/2" DIAMETER	EA	1		1		0	
680.730714	SIGNAL CABLE 7 CONDUCTORS, 14 AWG	LF	4920		1000		880	
680.77000105	MODIFY TRAFFIC SIGNAL INSTALLATION, LOCATION 1	ELOC	1		0		0	
680.77000205	MODIFY TRAFFIC SIGNAL INSTALLATION, LOCATION 2	ELOC	1		1		0	
680.78010005	ALTER PULLBOX FOR CONDUITS	EA	9		0		0	
680.79000105	REMOVE TRAFFIC SIGNAL INSTALLATION, LOCATION 1	ELOC	1		1		0	
680.79000205	REMOVE TRAFFIC SIGNAL INSTALLATION, LOCATION 2	ELOC	1		0		0	
680.80149305	NEMA TS 2 SOLID STATE MENU DRIVEN ACTUATED TRAFFIC SIGNAL CONTROLLER AND CABINET	EA	1		1		0	
680.813103	PEDESTRIAN SIGNAL SECTION - TYPE 1, 12 INCH	EA	74		16		16	
680.813105	PEDESTRIAN SIGNAL MODULE - 12 INCH, BI-MODAL HAND/MAN SYMBOLS, LED	EA	37		8		8	
680.81330010	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POLE	EA	40		8		8	
680.8141	PEDESTRIAN SIGNAL BRACKET MOUNT ASSEMBLY	EA	1		1		0	
680.8142	PEDESTRIAN SIGNAL POST TOP MOUNT ASSEMBLY	EA	20		3		4	
680.81500010	PEDESTRIAN COUNTDOWN TIMER MODULE	EA	37		8		8	
680.8204	OVERHEAD SIGN ASSEMBLY, TYPE D	EA	9		3		1	
680.8207	OVERHEAD SIGN ASSEMBLY, TYPE G	EA	1		0		0	
680.94000008	TRAFFIC SIGNAL SERVICE ENTRANCE	EA	1		1		0	
680.94997008	FURNISH AND INSTALL ELECTRICAL DISCONNECT GENERATOR TRANSFER SWITCH	EA	1		1		0	
680.95020615	SERVICE CABLE 2 CONDUCTOR NO. 06 AWG	LF	30		30		0	
680.95533211	METER CABINET	EA	1		1		0	
685.11120009	EPOXY REFLECTORIZED PAVEMENT STRIPES 20 MILS - HAND WORK	LF	6500		1610		1160	
697.03	FIELD CHANGE PAYMENT	DC	22000		0		0	
698.04	ASPHALT PRICE ADJUSTMENT	DC	100		0		0	
698.05	FUEL PRICE ADJUSTMENT	DC	100		0		0	

NOTE: BASE BID INCLUDES ALL 5 INTERSECTIONS. DEDUCT ALTERNATE #1 INCLUDES WORK AT THE OAK/BRINKERHOFF INTERSECTION. DEDUCT ALTERNATE #2 INCLUDES WORK AT THE CORNELIA/NORTH CATHERINE INTERSECTION.



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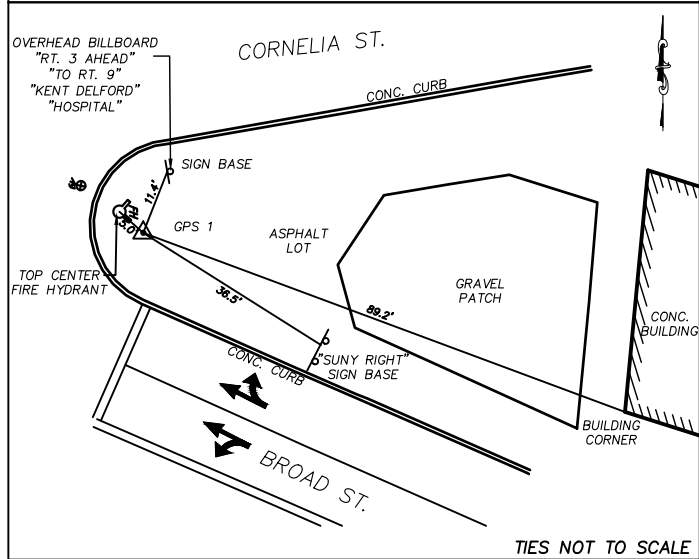
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REVISIONS		
PROJECT NO: A54.001.001		
DATE: DECEMBER 2020		
DRAWN BY: MNG		
DESIGNED BY: KAN		
CHECKED BY: TEH		
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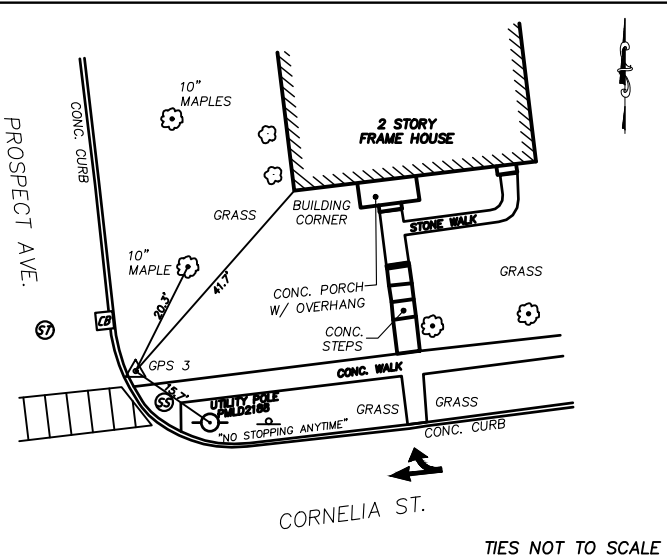
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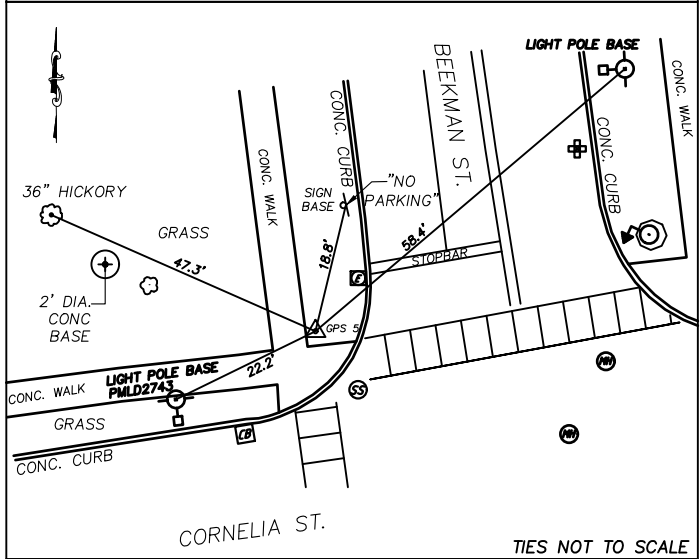
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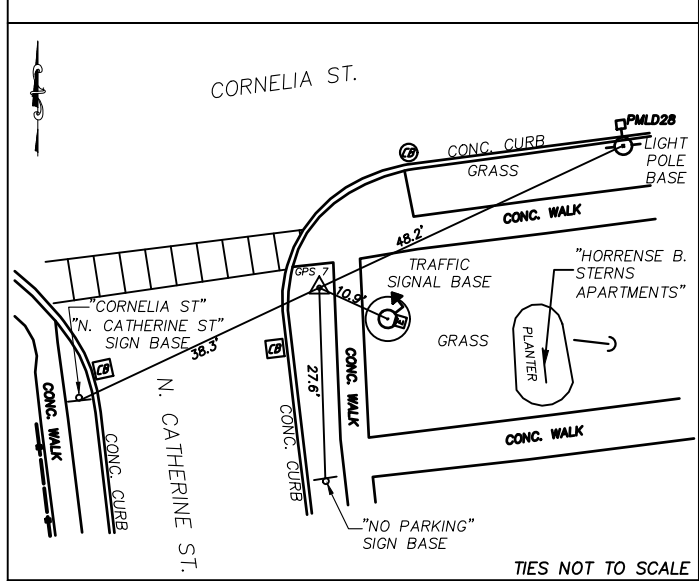
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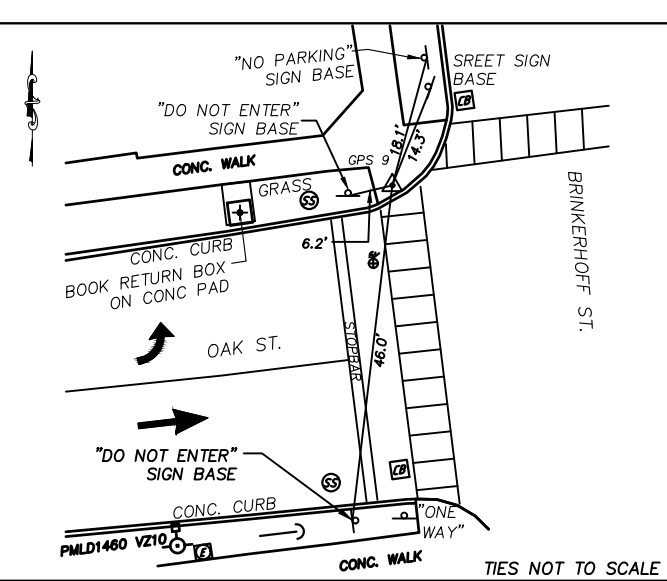
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GPS 9 – MAG NAIL



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GENERAL NOTES:

- COORDINATES ARE REFERENCED TO NORTH AMERICAN DATUM OF 1983, (NAD83/ 2011), NEW YORK STATE PLANE COORDINATE SYSTEM EAST ZONE (3101).
- THE VERTICAL DATUM REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988.
- CONTOUR INTERVAL = 1 FOOT.
- INFORMATION SHOWN HEREON IS FROM A FIELD SURVEY COMPLETED BY M.J. ENGINEERING AND LAND SURVEYING P.C. ON AUGUST 5TH THROUGH AUGUST 9TH, 2019.
- UNDERGROUND UTILITIES SHOWN HEREON WERE LOCATED BASED ON VISIBLE EVIDENCE AT GROUND LEVEL ONLY AND DO NOT PURPORT TO SHOW ALL UTILITIES, ANY AND ALL UTILITIES SHOWN ARE SUBJECT TO VERIFICATION BY EXCAVATION.
- UNAUTHORIZED ALTERATIONS OR ADDITION TO THIS SURVEY MAP IS A VIOLATION OF SECTION 7209 OF THE NEW YORK STATE EDUCATION LAW. COPIES OF THIS SURVEY MAP NOT BEARING THE LAND SURVEYORS SEAL AND SIGNED WITH INK SHALL NOT BE CONSIDERED TO BE VALID COPIES.
- TRAFFIC SIGNAL CONDUITS AND DETECTION LOOPS WERE DERIVED FROM THE CITY'S RECORD PLANS AND ADDED TO THE BASE MAPPING BY C&S ENGINEERS IN FEBRUARY 2020.

BENCHMARK TABLE			
NO.	ELEV.	DESCRIPTION	INTERSECTION
BM 1	242.678'	X-CUT FIRE HYDRANT NHOA	CORNELIA STREET AND BROAD STREET
BM 2	225.449'	X-CUT FIRE HYDRANT NHOA	CORNELIA STREET AND PROSPECT AVENUE
BM 3	172.327'	X-CUT FIRE HYDRANT EAST BOLT	CORNELIA STREET AND BEEKMAN STREET
BM 4	153.305'	EXISTING X-CUT FIRE HYDRANT SOUTHWEST BOLT	CORNELIA STREET AND N CATHERINE STREET
BM 5	147.372'	X-CUT FIRE HYDRANT NORTHWEST BOLT	OAK STREET AND BRINKERHOFF STREET



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BASELINE TIES &
BENCHMARKS

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GENERAL NOTES

- ALL WORK ZONE TRAFFIC CONTROL SHALL FOLLOW THE SPECIFIC REQUIREMENTS OF THE NYSDOT STANDARD SPECIFICATIONS, SECTION 619, THE STANDARD SHEETS LISTED ON THE COVER SHEET, AND THE TYPICAL APPLICATIONS SHOWN ON DRAWINGS WZTC-2 AND WZTC-3.
- THE CONTRACTOR SHALL MAINTAIN TRAFFIC THROUGHOUT THE LENGTH OF THE CONTRACT IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 OF THE CURRENT NYSDOT STANDARD SPECIFICATIONS, THE NATIONAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) WITH THE NEW YORK STATE (NYS) SUPPLEMENT AND AS ORDERED BY THE RESIDENT ENGINEER.
- THE TYPICAL DETAILS DEPICTED IN THE STANDARD SHEETS REFLECT THE MINIMUM REQUIREMENTS, UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
- PRIOR TO THE START OF ANY WORK OPERATIONS, ALL RELATED WORK FOR PROPOSED TEMPORARY TRAFFIC CONTROL SHALL BE COMPLETE. THIS INCLUDES BUT IS NOT LIMITED TO, ALL SIGNS, SIGNALS, PAVEMENT MARKINGS, BARRIERS, DELINEATION (CONES, DRUMS, ETC.), FLAGGERS, PAVEMENT MODIFICATIONS, AND ANY OTHER RELATED WORK AS DIRECTED BY THE RESIDENT ENGINEER.
- WEEDS, SHRUBBERY, CONSTRUCTION MATERIALS, EQUIPMENT, VEHICLES OR WORKERS SHALL NOT OBSCURE TRAFFIC CONTROL DEVICES OR OBSTRUCT TRAFFIC. THE CONTRACTOR SHALL BE REQUIRED TO TRIM ANY FOLIAGE OBSTRUCTING THE VISIBILITY OF ANY TRAFFIC CONTROL DEVICES WHETHER PERMANENT, TEMPORARY OR CONSTRUCTION.
- THE CONTRACTOR SHALL COORDINATE ANY WORK WITH OTHER CONTRACTORS, UTILITY COMPANIES, OR MUNICIPALITIES IN ORDER TO ENSURE ALL TRAFFIC IS PROPERLY AND SAFELY MAINTAINED AND PROTECTED AT ALL TIMES.
- PAYMENT FOR ALL WORK ZONE TRAFFIC CONTROL SHALL BE UNDER ITEM 619.01, WITH THE EXCEPTION OF TYPE III BARRICADES, WHICH SHALL BE PAID FOR UNDER ITEM 619.04.

SIGNS

- ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE NATIONAL MUTCD WITH NYS SUPPLEMENT. THE LOCATIONS OF THE SIGNS SHOWN ON THE NYSDOT STANDARD SHEETS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO THE APPROVAL OF THE RESIDENT ENGINEER.
- ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AOE. BLANK COVERS USED TO COVER PORTIONS OF EXISTING SIGNS SHALL BE OF A COLOR AND REFLECTORIZED MATERIAL MATCHING THAT OF THE SIGN BEING PARTIALLY COVERED. THE CONTRACTOR SHALL BE LIABLE FOR ANY DAMAGE TO SIGNS CAUSED BY THE METHODS USED TO TEMPORARILY REMOVE, RELOCATE OR COVER SIGN PANELS OR SIGN TEXT, AT NO ADDITIONAL COST TO THE CITY. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
- SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
- ALL CONSTRUCTION SIGNS SHALL BE MOUNTED ON NCHRP350 APPROVED TEMPORARY SIGN SUPPORTS UNLESS SHIELDED BY TEMPORARY CONCRETE BARRIER OR GUIDE RAIL, AND LOCATED BEYOND THE DEFLECTION DISTANCES.
- IF CONSTRUCTION RELATED TRAFFIC CONGESTION BACKS UP BEYOND THE INSTALLED ADVANCE SIGN SEQUENCE, ADDITIONAL ADVANCE SIGNING SHALL BE PLACED IN ADVANCE OF THE CONGESTION. REFER TO SECTION 619-3.02(H)(5) OF THE STANDARD SPECIFICATIONS REGARDING THE USE OF "BE PREPARED TO STOP" SIGNS.
- UNDER NO CIRCUMSTANCES SHALL A SIGN PANEL BE TRUNCATED OR TRIMMED SO THAT IT WILL NOT INTERFERE WITH TRAVEL WAY. IF THIS CONDITION EXISTS, THE RESIDENT ENGINEER WILL DIRECT THE CONTRACTOR TO RELOCATE THAT SIGN AS NOT TO CAUSE ANY OBSTRUCTION WITH THE TRAVEL WAY.
- WARNING FLAGS ON SIGNS MAY BE USED TO INCREASE TARGET VALUE AND VISIBILITY OF WORK ZONE SIGNS DURING DAYLIGHT HOURS.

DELINEATION AND GUIDING DEVICES

- WHERE POSSIBLE ALL DELINEATION AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2'-0" LATERAL CLEARANCE TO THE TRAVELED WAY.
- DEFORMED OR DAMAGED CHANNELIZING DEVICES THAT DO NOT MAINTAIN APPEARANCE, COLOR AND REFLECTIVITY WILL BE EVALUATED FOR ACCEPTABILITY IN ACCORDANCE WITH THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) "QUALITY GUIDELINES FOR WORK ZONE TRAFFIC CONTROL DEVICES". AS THE CONDITION OF DEVICES DETERIORATES OVER TIME, THOSE THAT FALL BELOW THE MARGINAL CATEGORY SHALL BE REPLACED.

LANE WIDTHS

- EXISTING LANE WIDTHS OF ROADWAYS SHOULD BE MAINTAINED THROUGH THE WORK ZONE TRAVELWAYS WHENEVER PRACTICAL. WHEN CONDITIONS REQUIRE A REDUCTION IN LANE WIDTH, THE MINIMUM WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE 10FT.

CONTRACTOR VEHICLES

- VEHICLES BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S WORKERS SHALL NOT BE PARKED ON THE PAVEMENT OR SHOULDERS ALONG THE ROADWAY WHERE PARKING IS NOT NORMALLY PERMITTED.
- VEHICLES BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED IN A MANNER WHICH OBSTRUCTS SIGNS, BARRIERS, BARRICADES, OTHER TRAFFIC CONTROL DEVICES.

LANE CLOSURES

- THE CONTRACTOR SHALL START SHORT TERM LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT PROJECT AND TRAFFIC CONDITIONS PERMIT.
- IF, IN THE OPINION OF THE ENGINEER, SHORT TERM LANE CLOSURES ARE CREATING EXCESSIVE DELAYS TO TRAFFIC OR ARE CREATING A SAFETY CONCERN DUE TO SLOW OR STOPPED TRAFFIC, AFFECTED LANE CLOSURES SHALL BE REMOVED AND THE ROADWAY OPENED TO TRAFFIC.
- THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS NOT WITHIN THE PROJECT LIMITS.

SIGNALIZED INTERSECTIONS

- WHEN LANE CLOSURE OPERATIONS AFFECT SIGNALIZED INTERSECTIONS, THE TRAFFIC SIGNAL SHALL BE TURNED OFF BEFORE PROCEEDING WITH THE FLAGGING OPERATION AND ANY EXISTING W3-3 SIGN COVERED.
- THE CONTRACTOR MUST RETURN THE SIGNAL TO A 3-COLOR OPERATION AT THE END OF THE WORK OPERATION. IF THE SIGNAL DOES NOT RETURN TO 3-COLOR OPERATION, THE CONTRACTOR SHALL NOTIFY THE CITY OF UTICA AND MUST CONTINUE FLAGGING UNTIL THE DPW ARRIVES AND THE SIGNAL IS REPAIRED.

FLAGGERS

- WHEN FLAGGERS ARE BEING USED FOR TRAFFIC CONTROL PURPOSES, FLAGGER SIGNS (W20-7) SHALL BE PLACED ON ALL APPROACHES TO THE AREAS BEING FLAGGGED. THE COST OF FLAGGER SIGNS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL. FLAGGER SIGNS SHALL NOT BE USED FOR BRIEF PERIODS OF INCIDENTAL FLAGGING. FLAGGER SIGNS SHALL NOT BE VISIBLE WHEN FLAGGERS ARE NOT BEING USED.
- STOP/SLOW PADDLES SHALL BE USED FOR FLAGGING AND SHALL CONFORM TO THE REQUIREMENTS IN THE MUTCD BUT SHALL BE A MINIMUM OF 2'-0". THE COST OF THESE PADDLES AND ALL FLAGGERS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.
- FLAGGERS SHALL NOT POSITION THEMSELVES DIRECTLY IN THE PATH OF ONCOMING TRAFFIC. THE FLAGGER SHOULD STAND EITHER ON THE SHOULDER ADJACENT TO THE TRAFFIC BEING CONTROLLED OR IN THE CLOSED/CHANNELIZED LANE. AT ALL TIMES, THE CONTRACTOR SHALL PROVIDE FLAGGERS WITH A CLEAR ESCAPE PATH, FREE OF PARKED VEHICLES, EQUIPMENT OR OTHER OBSTACLES.
- THE CONTRACTOR SHALL PROVIDE FLAGGERS WHERE SIGHT DISTANCES ARE IMPAIRED BY THE OPERATION OR IN ANY OTHER SITUATION AOE.
- FLAGGERS SHALL BE REQUIRED TO USE TWO-WAY RADIOS, WALKIE-TALKIES, OR OTHER FORMS OF ENHANCED COMMUNICATION WHEN ONE FLAGGER IS NOT VISIBLE TO THE OTHER OR IF THE ENGINEER DEEMS IT NECESSARY. ALL COSTS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.
- FLAGGERS SHALL USE THE FREE HAND FOR EMPHASIS AS SHOWN IN PART 6 OF THE MUTCD. EACH FLAGGER SHALL CONTROL NO MORE THAN ONE APPROACH OF TRAFFIC UNLESS THE APPROACHES PRESENT UNUSUALLY LOW SPEEDS AND/OR UNUSUALLY LOW VOLUMES, WITH ADEQUATE SIGHT DISTANCE FOR SAFE HANDLING OF TRAFFIC, AS DETERMINED BY THE ENGINEER.
- FLAGGER STATIONS SHOULD BE VISIBLE FAR ENOUGH AHEAD TO PERMIT ALL VEHICLES TO STOP. THE FLAGGER SHOULD BE STATIONED FAR ENOUGH AHEAD OF THE WORK TO WARN THEM (FOR EXAMPLE WITH HORNS, WHISTLES, ETC.) OF APPROACHING DANGER, SUCH AS VEHICLES OUT OF CONTROL.
- WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES AND FLAGGERS SHALL BE PROVIDED.

BICYCLES AND PEDESTRIANS

- THE NEEDS AND CONTROL OF ALL ROAD USERS (MOTORISTS, BICYCLISTS, AND PEDESTRIANS WITHIN THE HIGHWAY INCLUDING PERSONS WITH DISABILITIES IN ACCORDANCE WITH THE AMERICAN WITH DISABILITIES ACT OF 1990 (ADA) TITLE II, PARAGRAPH 35.130) THROUGH A WORK ZONE SHALL BE AN ESSENTIAL PART OF THE HIGHWAY CONSTRUCTION, UTILITY WORK, MAINTENANCE, AND THE MANAGEMENT OF TRAFFIC INCIDENTS.
- THE CONTRACTOR SHALL PROVIDE, DELINEATE AND MAINTAIN A USABLE TRAVEL PATH FOR PEDESTRIANS AND BICYCLISTS EITHER THROUGH OR AROUND WORK ZONES FOR THE DURATION OF THIS PROJECT. THE TRAVEL PATH SHALL COMPLY WITH THE PROVISIONS OF THE ADA GUIDELINES AND NYS HIGHWAY LAW, SECTION 316. THE CONTRACTOR MAY UTILIZE ANY COMBINATION OF THE FOLLOWING TO PROVIDE THE USABLE TRAVEL PATH: EXISTING SIDEWALKS/SHOULDERS; TEMPORARY SIDEWALKS/SHOULDERS; FINAL SIDEWALKS/SHOULDERS; AND EXISTING OR TEMPORARY SIDEWALK CURB RAMPS. THE USABLE TRAVEL PATH SHALL BE CLEARLY MARKED AND STAGED BETWEEN INTERSECTING ROADS TO ENSURE SAFE PEDESTRIAN CROSSING AND ACCESSIBILITY. PEDESTRIAN ACCESS TO RESIDENTIAL AND COMMERCIAL PROPERTIES SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION. PAYMENT WILL BE MADE UNDER ITEM 619.01.
- THE CONTRACTOR SHALL CONDUCT WORK OPERATIONS TO MINIMIZE THE INTERRUPTION OF PEDESTRIAN TRAFFIC. THE TEMPORARY STORAGE OF CONSTRUCTION EQUIPMENT OR MATERIALS SHALL NOT BE PERMITTED ON ANY USABLE TRAVEL PATH THAT IS OPEN TO PEDESTRIAN TRAFFIC.
- THE INSTALLATION OF TEMPORARY/FINAL SIDEWALK SHALL BE STAGED SO THAT WORK IS STARTED AND COMPLETED ON ONE SIDE OF THE ROAD PRIOR TO THE START OF WORK ON THE OPPOSITE SIDE OF THE ROAD. WHEN A SECTION OF THE SIDEWALK/USABLE TRAVEL PATH MUST BE CLOSED AND PEDESTRIAN TRAFFIC IS SHIFTED TO THE SIDEWALK/USABLE TRAVEL PATH ON THE OPPOSITE SIDE OF THE ROAD, PEDESTRIANS SHALL BE DIRECTED TO CROSS AT THE NEXT AVAILABLE INTERSECTION.

NIGHTTIME CONSTRUCTION

- NO NIGHT WORK WILL BE ALLOWED UNLESS PRIOR APPROVAL IS GIVEN BY THE CITY. ADDITIONAL WORK ZONE TRAFFIC CONTROL WILL BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.

PAVEMENT MARKINGS

- EXISTING MARKINGS, LETTERS AND SYMBOLS WILL BE RE-APPLIED AS NECESSARY, AOE. PAVEMENT MARKINGS THAT HAVE BEEN OBLITERATED BY THE OPERATION OF UTILITY COMPANIES SHALL BE MAINTAINED AS ORDERED BY THE RESIDENT ENGINEER. PAYMENT WILL BE MADE UNDER ITEM 619.01.

NYSDOT STANDARD SHEETS/TEMPORARY TRAFFIC CONTROL

- THE CONTRACTOR SHALL PROVIDE APPROPRIATE WZTC USING THE NYSDOT STANDARD SHEETS LISTED ON THE COVER SHEET.
- INGRESS AND EGRESS OF CONTRACTOR'S TRUCKS AND EQUIPMENT MAY REQUIRE SPOTTERS AND FLAGGERS A.O.B.E.
- MOST WORK CAN BE ACCOMPLISHED WITH DAILY INTERSECTION LANE CLOSURES PER NYSDOT STANDARD SHEET 619-61.
- IF THE CONTRACTOR'S OPERATIONS ENCROACH INTO AN INTERSECTING STREET, UTILIZE SIDEWALK AND SHOULDER CLOSURES, FOLLOWED BY LANE CLOSURES WITH FLAGGERS, AS APPROPRIATE. WORK NEAR INTERSECTIONS SHALL REQUIRE ADDITIONAL WZTC PER STANDARD SHEET 619-61.
- IF SIDEWALK CLOSURES AND DETOURS CANNOT BE PROVIDED THE CONTRACTOR SHALL EMPLOY A SPOTTER TO ESCORT PEDESTRIANS THROUGH THE ACTIVE WORK AREA. PROVIDE A DELINEATED TRAVERSIBLE PATH DURING NON-WORKING HOURS.

PUBLIC ACCESS

- ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES, UNLESS OTHER AGREEMENTS SUITABLE TO THE OWNERS, CAN BE MADE. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVE. THE CONTRACTOR SHALL MAINTAIN ACCESS TO COMMERCIAL DRIVEWAYS AT ALL TIMES WHEN A FACILITY IS IN USE. FOR MULTIPLE ACCESS PROPERTIES, ONLY ONE DRIVEWAY MAY BE CLOSED AT ONE TIME. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
- SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA. THE MATERIAL SHALL CONSIST OF CRUSHED STONE OR ASPHALT CONCRETE, WHICHEVER IS DEEMED MORE APPROPRIATE FOR THE CIRCUMSTANCES, AOE. PLASTIC DRUMS OR OTHER APPROVED TRAFFIC CONTROL DEVICES SHALL BE USED TO DELINEATE THE DRIVEWAY AND TO CONTROL ACCESS DURING NON-WORKING HOURS.



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WORK ZONE
TRAFFIC CONTROL
NOTES

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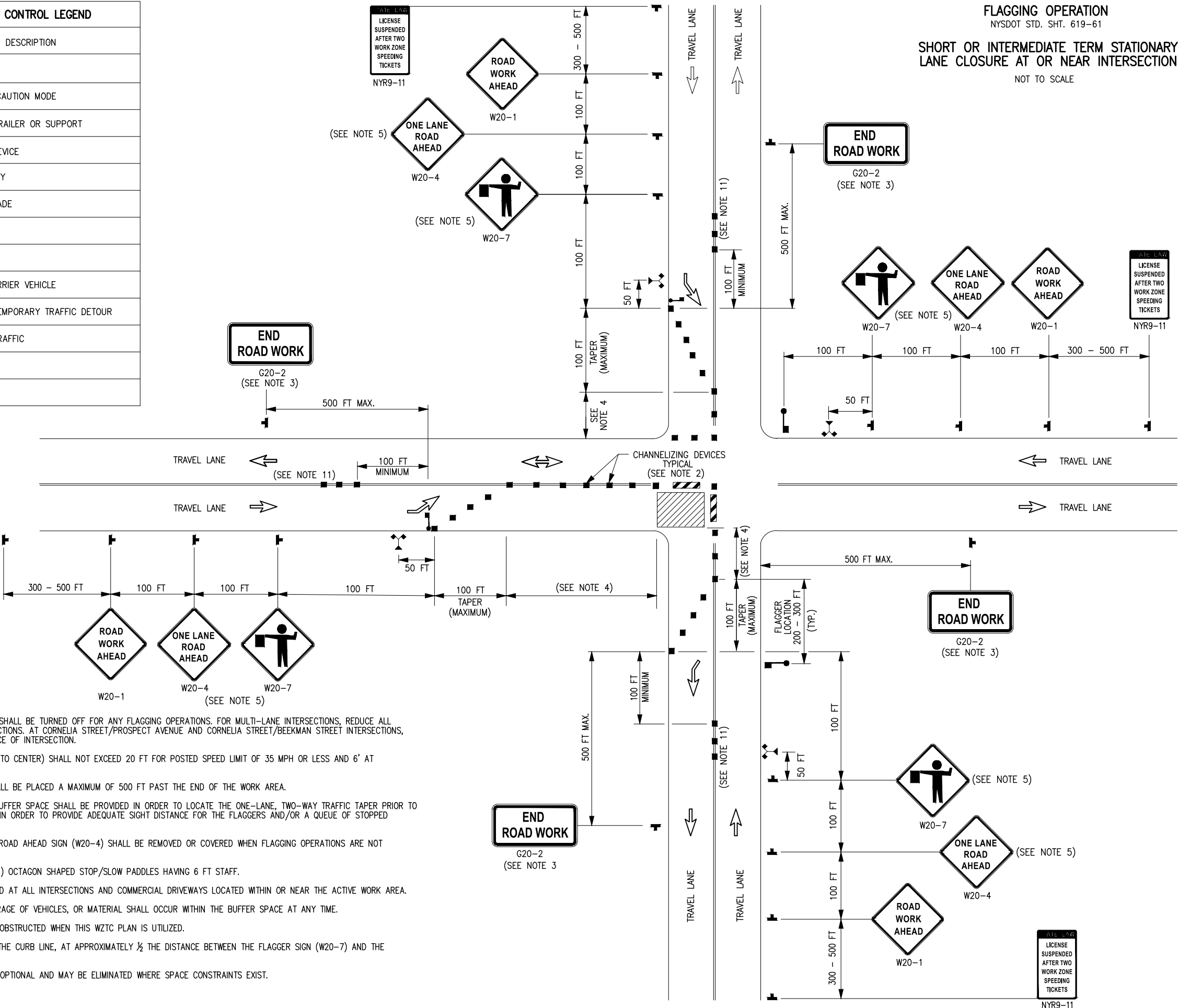
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WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANNELIZING DEVICE
	SIGN, TEMPORARY
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK SPACE
	SHADOW OR BARRIER VEHICLE
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	FLAGGER
	FLAG TREE

NOTES:

1. AT SIGNALIZED INTERSECTIONS, SIGNALS SHALL BE TURNED OFF FOR ANY FLAGGING OPERATIONS. FOR MULTI-LANE INTERSECTIONS, REDUCE ALL APPROACHES TO ONE LANE IN ALL DIRECTIONS. AT CORNELIA STREET/PROSPECT AVENUE AND CORNELIA STREET/BEEKMAN STREET INTERSECTIONS, CLOSE OFF LEFT TURN LANES IN ADVANCE OF INTERSECTION.
2. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20 FT FOR POSTED SPEED LIMIT OF 35 MPH OR LESS AND 6' AT INTERSECTIONS AND DRIVEWAYS.
3. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500 FT PAST THE END OF THE WORK AREA.
4. WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
5. FLAGGER SIGN (W20-7) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED OR COVERED WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
6. ALL FLAGGERS SHALL USE 24 INCH (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6 FT STAFF.
7. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK AREA.
8. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
9. SIDEWALKS SHALL REMAIN OPEN AND UNOBSTRUCTED WHEN THIS WZTC PLAN IS UTILIZED.
10. THE FLAG TREE SHALL BE LOCATED AT THE CURB LINE, AT APPROXIMATELY ½ THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7) AND THE FLAGGER.
11. CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.



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LOCAL PEDESTRIAN SAFETY
ACTION PLAN
CITY OF PLATTSBURGH
CLINTON COUNTY, NEW YORK

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: A54.001.001		
DATE: DECEMBER 2020		
DRAWN BY: MNG		
DESIGNED BY: KAN		
CHECKED BY: TEH		
NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION 2 OF THE NEW YORK EDUCATION LAW		

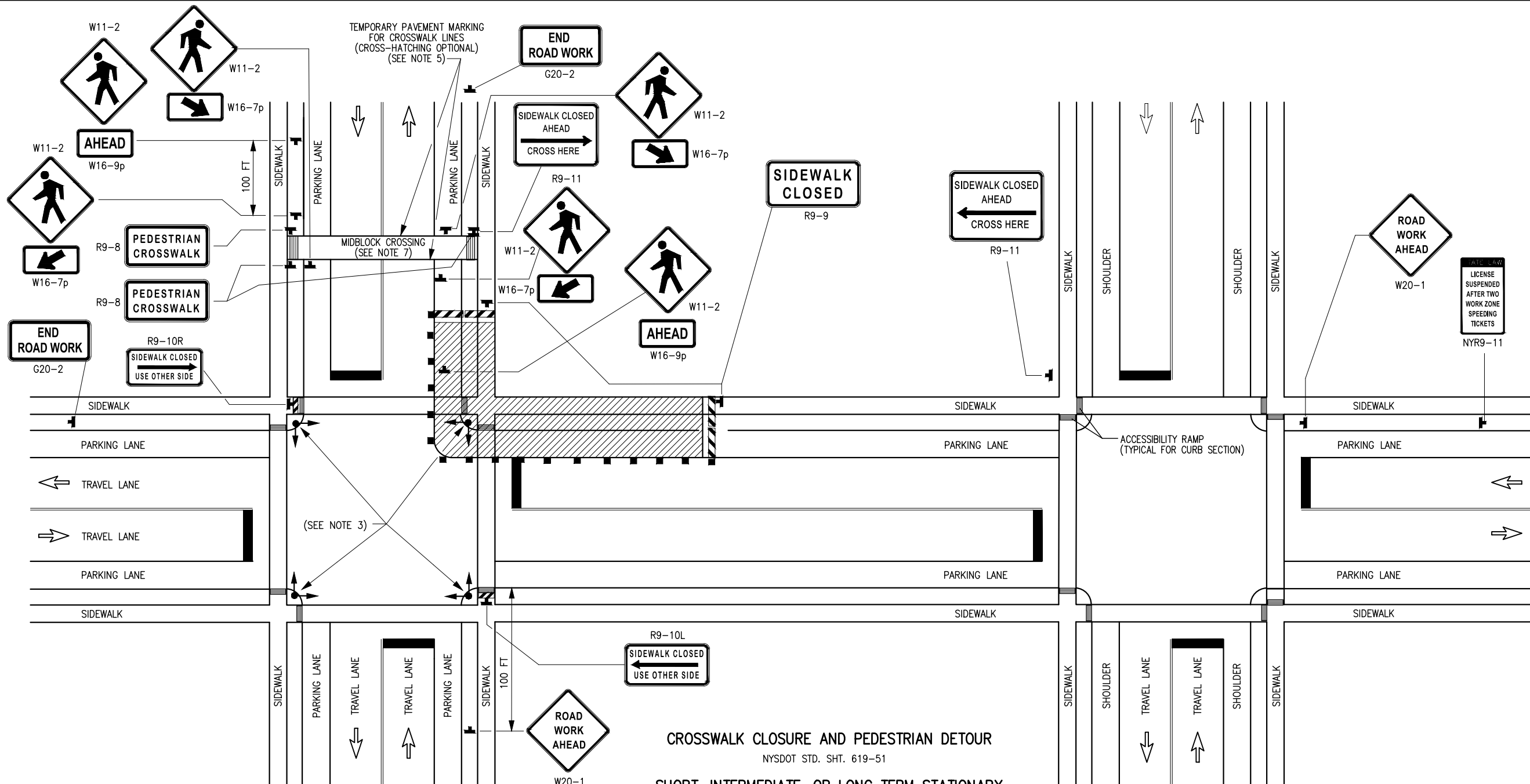
WORK ZONE
TRAFFIC
CONTROL PLAN

WZTC-2

SHEET 7 OF 20

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- NOTES:
- MIDBLOCK CROSSING AND INTERSECTION CROSSING ARE TWO DIFFERENT SOLUTIONS, BUT SHOWN ON THE SAME DETAIL AS OPTIONS.
 - WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING FACILITY.
 - CURB PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FT IN ADVANCE OF THE MID-BLOCK CROSSWALK.
 - PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHALL BE COVERED OR DEACTIVATED.
 - FOR NIGHTTIME CLOSURES, FLASHING WARNING LIGHTS SHALL BE USED ON BARRICADES SUPPORTING SIGNS AND CLOSING SIDEWALKS. ILLUMINATION LEVEL 2 ARE REQUIRED FOR R9-8, R9-10 AND R9-11 SIGNS PER SECTION 619 OF THE STANDARD SPECIFICATIONS.
 - FOR LONG-TERM STATIONARY WORK, THE DOUBLE YELLOW CENTERLINE AND/OR LANE LINES SHALL BE COVERED BETWEEN THE TEMPORARY CROSSWALK LINES.
 - AREA SHALL BE RECONSTRUCTED TO BE ACCESSIBLE TO PEDESTRIANS.
 - ALL MID-BLOCK CROSSINGS REQUIRE THE APPROVAL OF THE CITY DPW.
 - TYPE III BARRICADES MAY BE SUBSTITUTED FOR TYPE II BARRICADES AS PER 619 STANDARD SPECIFICATIONS.
 - ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES SUCH AS LANE CLOSURE SIGNING OR ROAD NARROWS SIGN (W5-4) MAY BE REQUIRED.

CROSSWALK CLOSURE AND PEDESTRIAN DETOUR
NYSDOT STD. SHT. 619-51
SHORT, INTERMEDIATE, OR LONG TERM STATIONARY
CROSSWALK CLOSURE ON 2-LANE 2-WAY ROADWAY
NOT TO SCALE

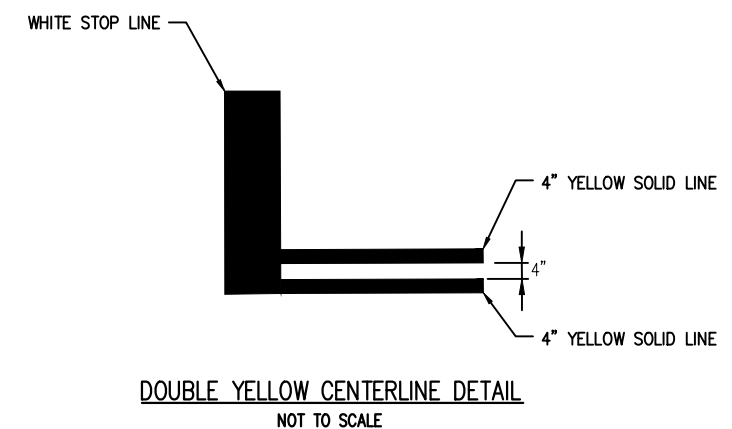
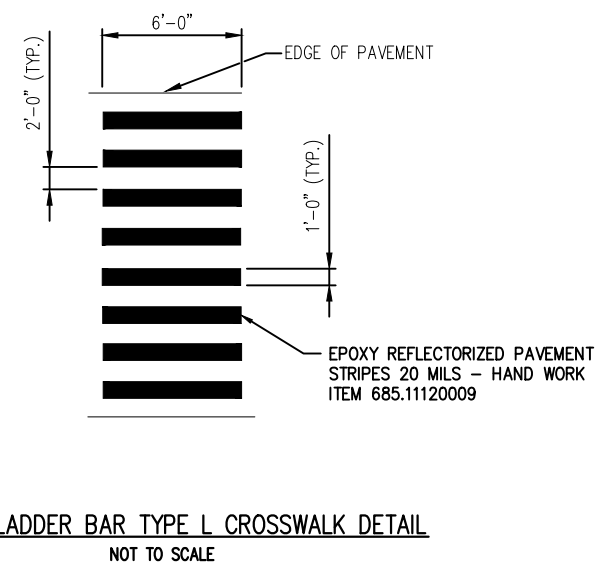
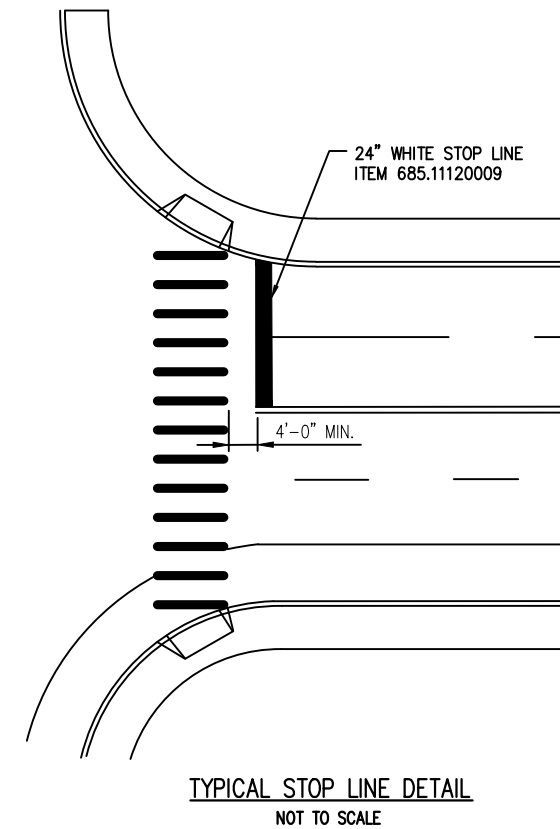
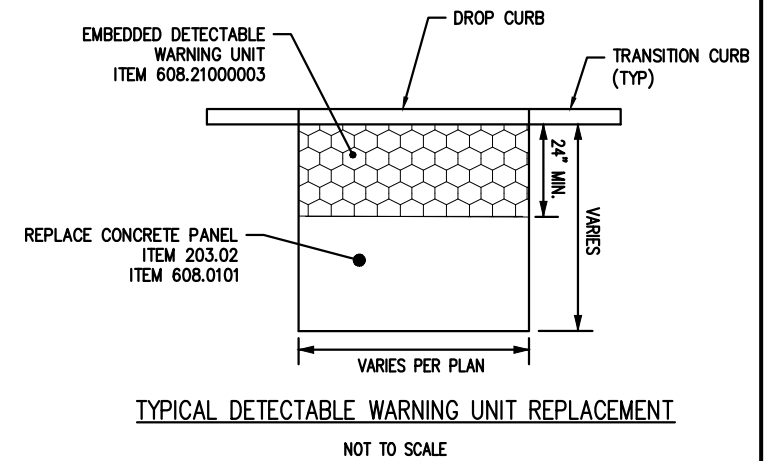
WORK ZONE TRAFFIC CONTROL LEGEND			
SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
	ARROW PANEL		WORK SPACE
	ARROW PANEL, CAUTION MODE		SHADOW OR BARRIER VEHICLE
	ARROW PANEL TRAILER OR SUPPORT		DIRECTION OF TEMPORARY TRAFFIC DETOUR
	CHANNELIZING DEVICE		DIRECTION OF TRAFFIC
	SIGN, TEMPORARY		FLAGGER
	TYPE III BARRICADE		FLAG TREE
	WARNING LIGHTS		

LOCAL PEDESTRIAN SAFETY
ACTION PLAN
CITY OF PLATTSBURGH
CLINTON COUNTY, NEW YORK

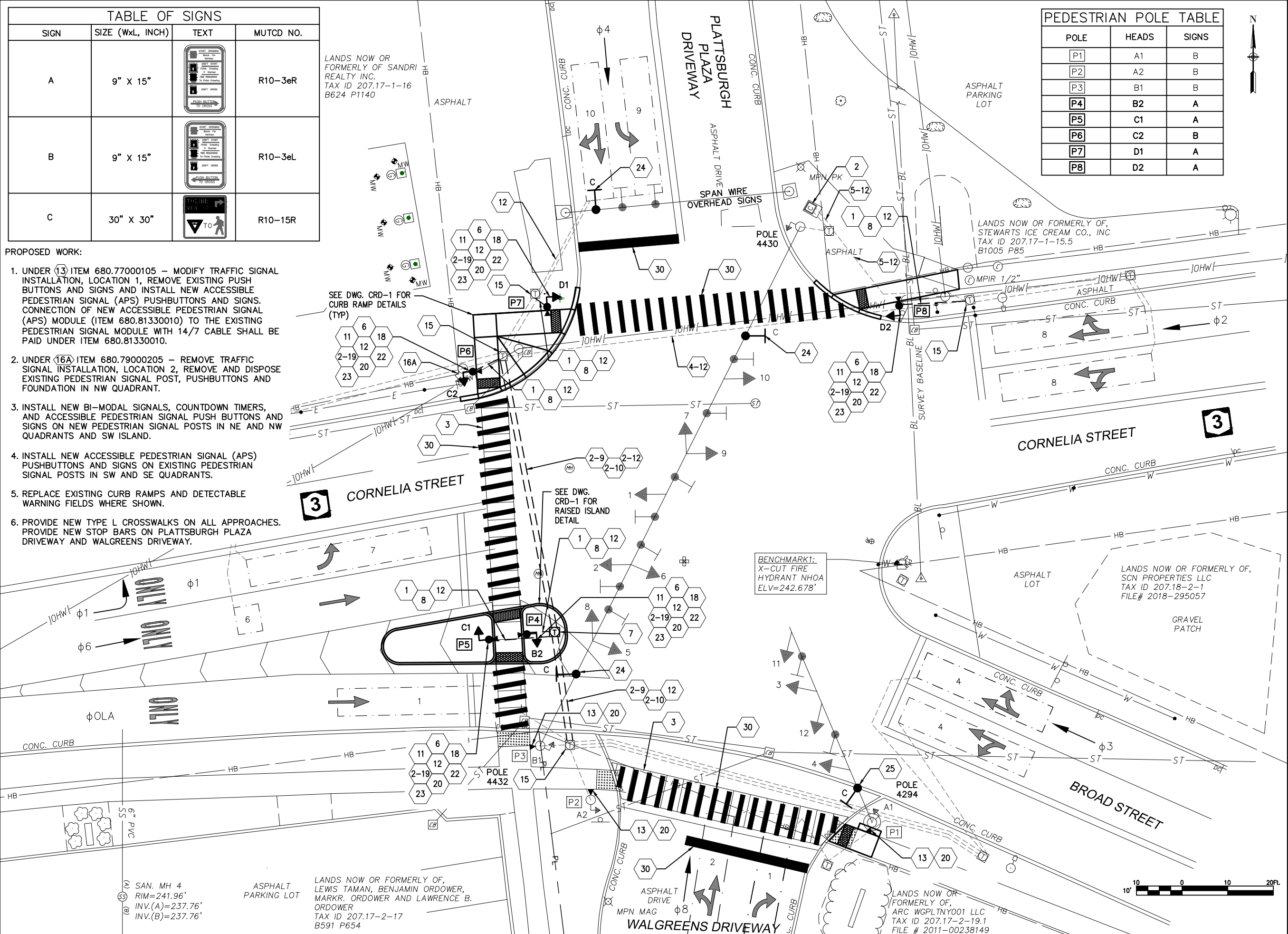
MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO:	A54.001.001	
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WORK ZONE
TRAFFIC
CONTROL PLAN

1. THE CONTRACTOR SHALL INSTALL NEW SIDEWALKS, CURB RAMPS, AND CAST IRON EMBEDDED DETECTABLE WARNING UNITS THAT MEET ADA STANDARDS, AS SHOWN ON THE PLANS. THE NEW SIDEWALK SHALL CONNECT FLUSH TO THE EXISTING SIDEWALKS.
2. CURB RAMPS SHALL BE BUILT TO AND FOLLOW THE NYSDOT 608-01 STANDARD SHEETS.
3. THE SIDEWALK DEPTH SHALL BE 6" FOR RADIUS SLABS AND 4" FOR ALL OTHER SLABS.
4. ITEM 610.16010020 SHALL BE USED ADJACENT TO NEW SIDEWALK TO MATCH EXISTING CONDITIONS.
5. PEDESTRIAN POLES SHALL BE INSTALLED ADJACENT TO SIDEWALKS AND LANDINGS.
6. SEE DWGS. CRD-1 TO CRD-2 FOR CURB RAMP LAYOUTS.
7. THE DETAILS AND DIMENSIONS PROVIDED ARE APPROXIMATE TO ACHIEVE ADA COMPLIANT RAMPS. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES TO ENSURE COMPLIANCE WITH ADA REQUIREMENTS. SEE STANDARD SHEET 608-01 FOR CROSS SLOPE REQUIREMENTS ON ALL SIDEWALKS AND RAMPS.
8. IF LAYOUTS REQUIRE ADJUSTMENT DUE TO FIELD CONDITIONS, NOTIFY ENGINEER IMMEDIATELY.
9. SIDEWALK REMOVALS SHALL END AT AN EXISTING JOINT.
10. ARROWS ON RAMPS REPRESENT DOWN GRADE DIRECTION. SEE STANDARD SHEET 608-01 FOR RUNNING SLOPE REQUIREMENTS OF A CURB RAMP.
11. USE RADIAL CAST IRON DETECTABLE WARNING UNITS IF MANUFACTURED AT THE RADIUS OF THE CURB ABUTTING THE UNIT. DETECTABLE WARNING FIELDS COMPRISED OF CUT AND SPLICED SECTIONS SHALL NOT BE PERMITTED.
12. AREAS OF PERMANENT SIDEWALK REMOVAL ARE TO BE RESTORED WITH ITEM 610.16010020. IN GRASS AREAS, THE PERIMETER AREA AT PROPOSED SIDEWALK REMOVAL AND REPLACEMENT SHALL BE RESTORED WITH A 2'-0" STRIP OF ITEM 610.16010020.
13. ALL DETECTABLE WARNING SURFACES SHALL BE 2'-0" BY 5'-0" MINIMUM.
14. NEW CURB SHALL BE FLUSH ACROSS CURB RAMP.
15. THE EXCAVATION DEPTH WILL BE THE DEPTH FROM THE TOP OF ROAD TO THE TOP OF EXISTING SUBBASE. ALL EXISTING PAVEMENT LAYERS ARE TO BE REMOVED AND REPLACED WITH ITEM 304.12.



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LOCAL PEDESTRIAN SAFETY ACTION PLAN
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CLINTON COUNTY, NEW YORK

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TRAFFIC SIGNAL PLAN - CORNELIA STREET AND BROAD STREET

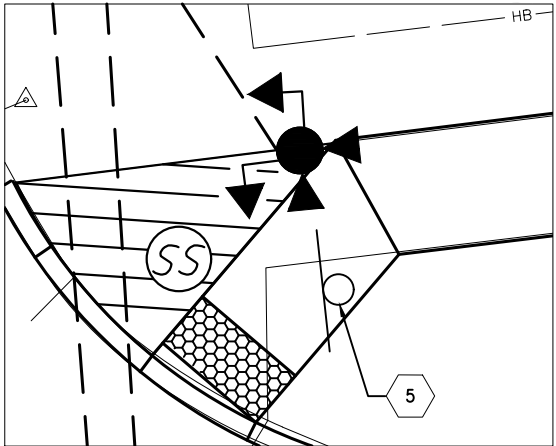
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TABLE OF SIGNS			
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B	9" X 15"		R10-3eL
C	30" X 30"		R10-15R

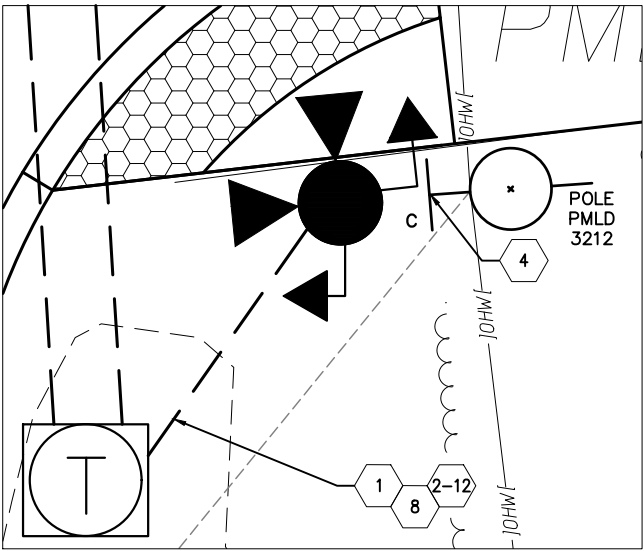
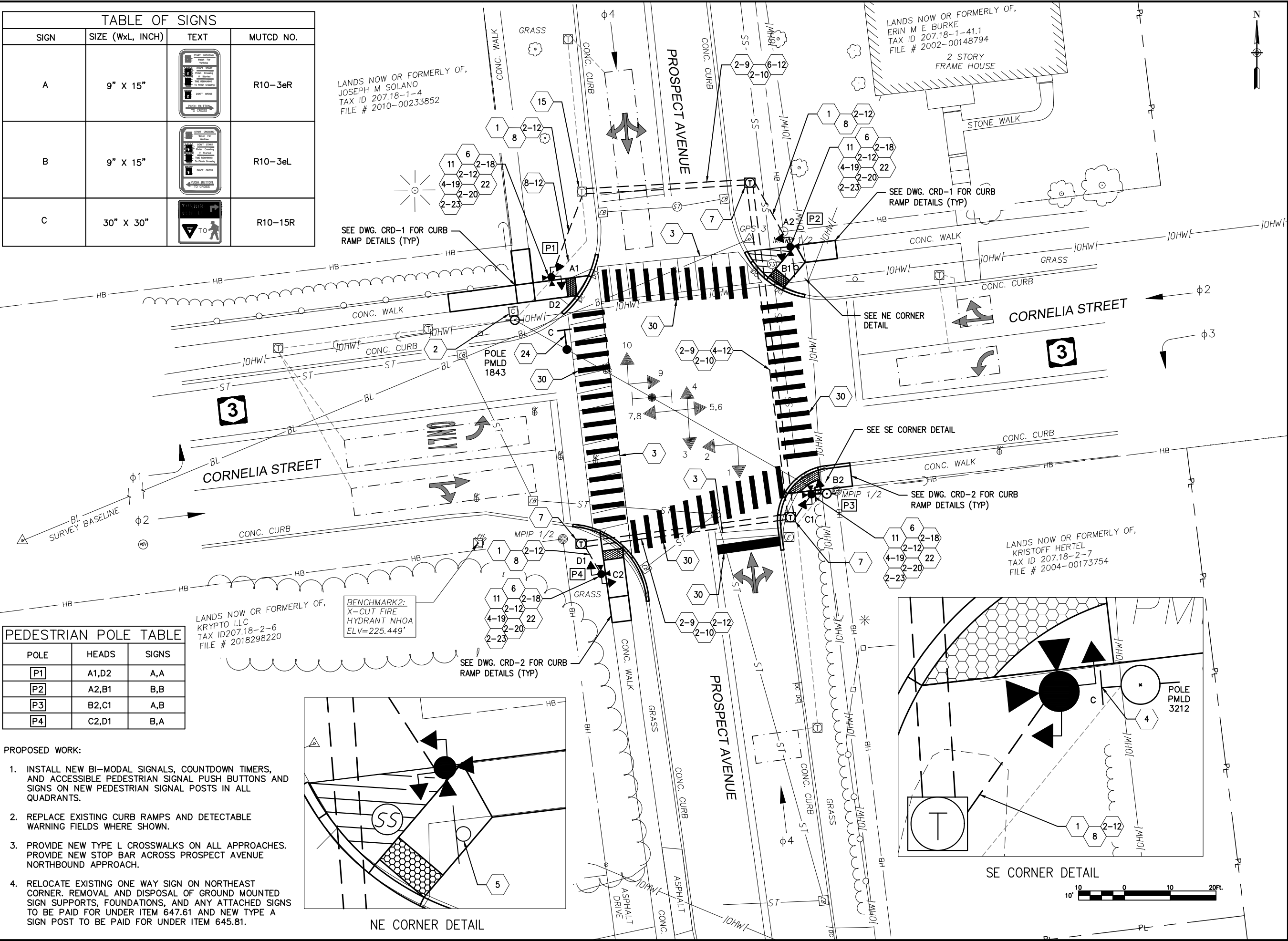
PEDESTRIAN POLE TABLE		
POLE	HEADS	SIGNS
P1	A1,D2	A,A
P2	A2,B1	B,B
P3	B2,C1	A,B
P4	C2,D1	B,A

PROPOSED WORK:

1. INSTALL NEW BI-MODAL SIGNALS, COUNTDOWN TIMERS, AND ACCESSIBLE PEDESTRIAN SIGNAL PUSH BUTTONS AND SIGNS ON NEW PEDESTRIAN SIGNAL POSTS IN ALL QUADRANTS.
2. REPLACE EXISTING CURB RAMP AND DETECTABLE WARNING FIELDS WHERE SHOWN.
3. PROVIDE NEW TYPE L CROSSWALKS ON ALL APPROACHES. PROVIDE NEW STOP BAR ACROSS PROSPECT AVENUE NORTHBOUND APPROACH.
4. RELOCATE EXISTING ONE WAY SIGN ON NORTHEAST CORNER. REMOVAL AND DISPOSAL OF GROUND MOUNTED SIGN SUPPORTS, FOUNDATIONS, AND ANY ATTACHED SIGNS TO BE PAID FOR UNDER ITEM 647.61 AND NEW TYPE A SIGN POST TO BE PAID FOR UNDER ITEM 645.81.



NE CORNER DETAIL



SE CORNER DETAIL



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LOCAL PEDESTRIAN SAFETY
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CITY OF PLATTSBURGH
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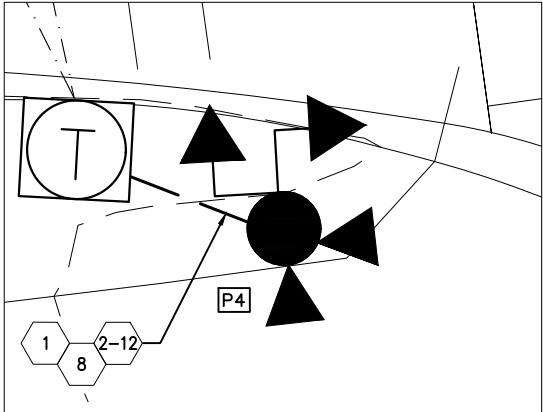
TRAFFIC SIGNAL
PLAN -
CORNELIA STREET
AND
PROSPECT AVENUE

TSP-2

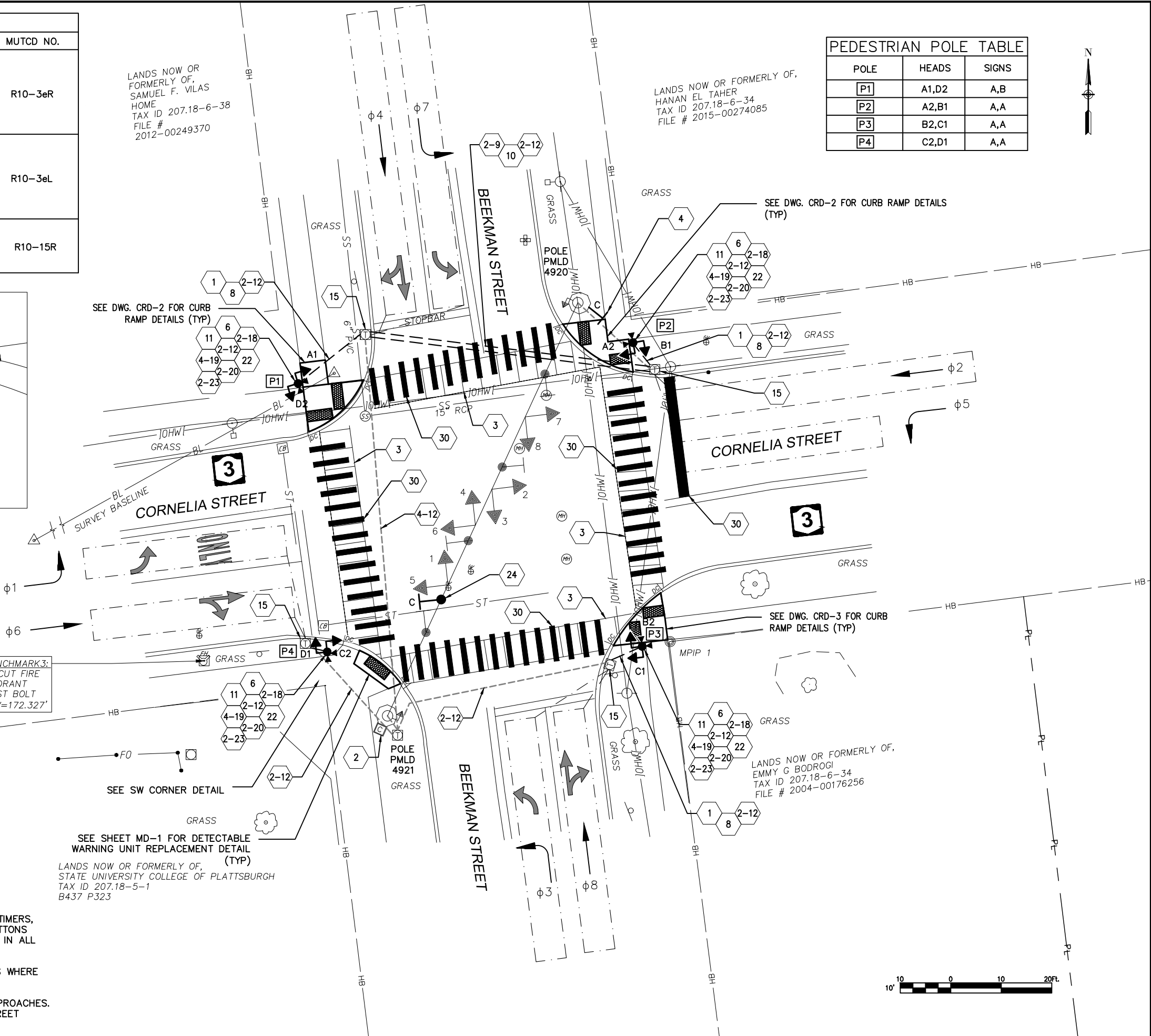
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A	9" X 15"		R10-3eR
B	9" X 15"		R10-3eL
C	30" X 30"		R10-15R



SW CORNER DETAIL



SEE SHEET MD-1 FOR DETECTABLE WARNING UNIT REPLACEMENT DETAIL (TYP)

LANDS NOW OR FORMERLY OF,
STATE UNIVERSITY COLLEGE OF PLATTSBURGH
TAX ID 207.18-5-1
B437 P323

LANDS NOW OR FORMERLY OF,
HANAN EL TAHER
TAX ID 207.18-6-34
FILE # 2015-00274085

PEDESTRIAN POLE TABLE		
POLE	HEADS	SIGNS
P1	A1,D2	A,B
P2	A2,B1	A,A
P3	B2,C1	A,A
P4	C2,D1	A,A



PROPOSED WORK:

1. INSTALL NEW BI-MODAL SIGNALS, COUNTDOWN TIMERS, AND ACCESSIBLE PEDESTRIAN SIGNAL PUSH BUTTONS AND SIGNS ON NEW PEDESTRIAN SIGNAL POSTS IN ALL QUADRANTS.
2. REPLACE EXISTING DETECTABLE WARNING FIELDS WHERE SHOWN.
3. PROVIDE NEW TYPE L CROSSWALKS ON ALL APPROACHES. PROVIDE NEW STOP BAR ACROSS CORNELIA STREET WESTBOUND APPROACH.



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LOCAL PEDESTRIAN SAFETY
ACTION PLAN
CITY OF PLATTSBURGH
CLINTON COUNTY, NEW YORK

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TRAFFIC SIGNAL
PLAN -
CORNELIA STREET
AND
BEEKMAN STREET

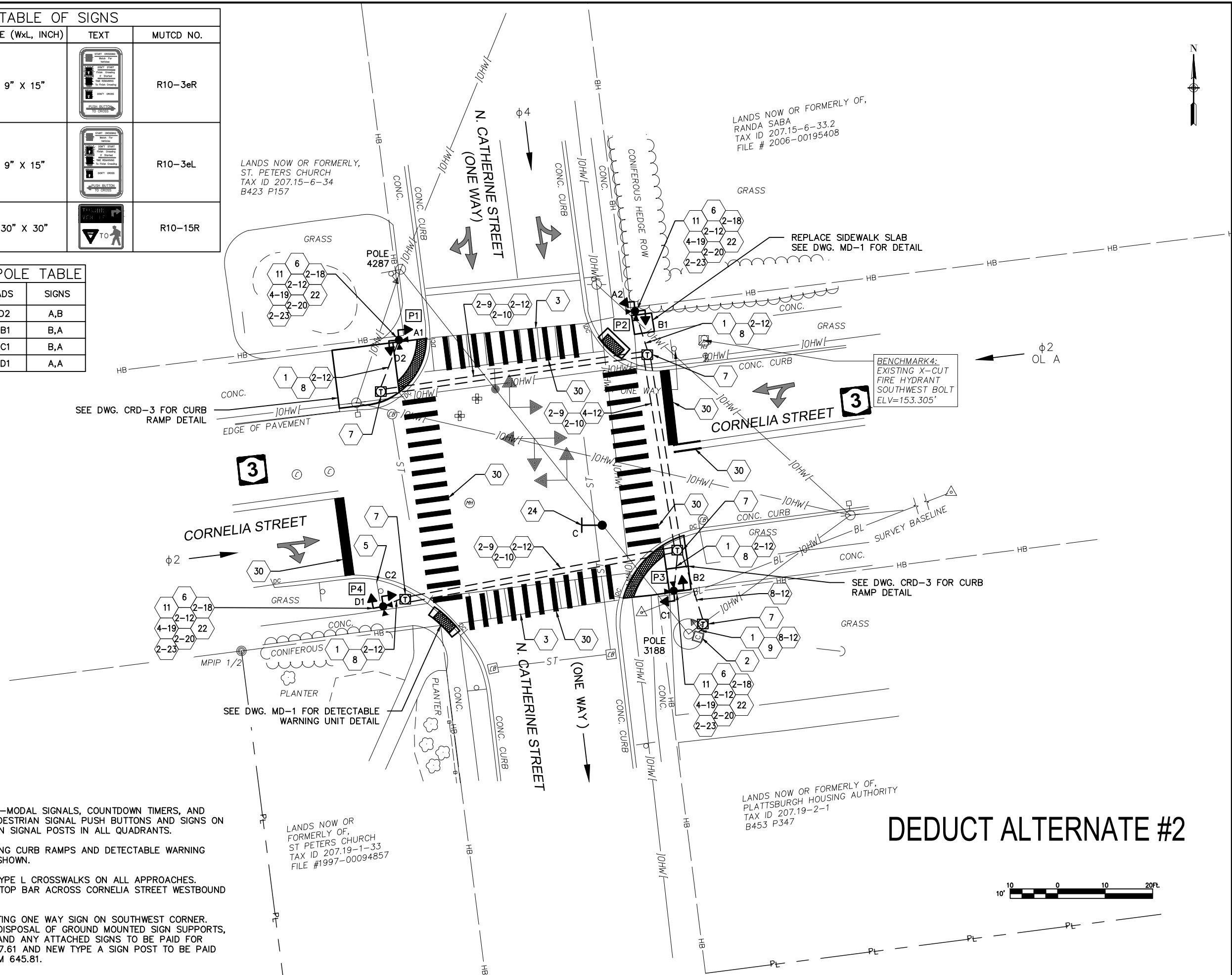
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SHEET 12 OF 20

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TABLE OF SIGNS			
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B	9" X 15"		R10-3eL
C	30" X 30"		R10-15R

PEDESTRIAN POLE TABLE		
POLE	HEADS	SIGNS
P1	A1,D2	A,B
P2	A2,B1	B,A
P3	B2,C1	B,A
P4	C2,D1	A,A



- PROPOSED WORK:
- INSTALL NEW BI-MODAL SIGNALS, COUNTDOWN TIMERS, AND ACCESSIBLE PEDESTRIAN SIGNAL PUSH BUTTONS AND SIGNS ON NEW PEDESTRIAN SIGNAL POSTS IN ALL QUADRANTS.
 - REPLACE EXISTING CURB RAMPS AND DETECTABLE WARNING FIELDS WHERE SHOWN.
 - PROVIDE NEW TYPE L CROSSWALKS ON ALL APPROACHES. PROVIDE NEW STOP BAR ACROSS CORNELIA STREET WESTBOUND APPROACH.
 - RELOCATE EXISTING ONE WAY SIGN ON SOUTHWEST CORNER. REMOVAL AND DISPOSAL OF GROUND MOUNTED SIGN SUPPORTS, FOUNDATIONS, AND ANY ATTACHED SIGNS TO BE PAID FOR UNDER ITEM 647.61 AND NEW TYPE A SIGN POST TO BE PAID FOR UNDER ITEM 645.81.

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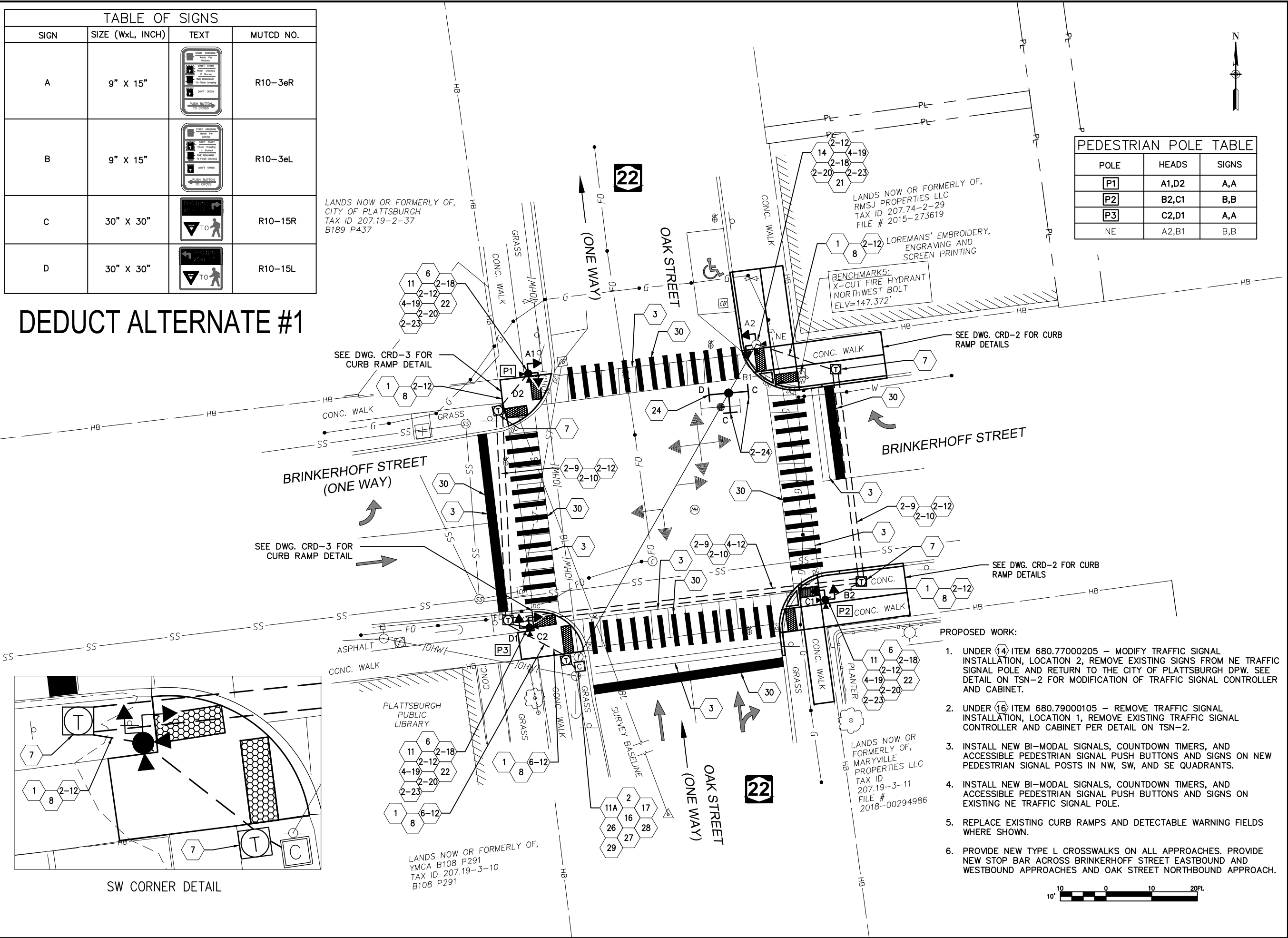
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**TRAFFIC SIGNAL
PLAN -
CORNELIA STREET
AND
N CATHERINE ST**

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B	9" X 15"		R10-3eL
C	30" X 30"		R10-15R
D	30" X 30"		R10-15L

DEDUCT ALTERNATE #1



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TRAFFIC SIGNAL
PLAN -
OAK STREET AND
BRINKERHOFF
STREET

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TRAFFIC SIGNAL POLES

- ANCHOR BOLT COVERS SHALL BE INSTALLED ON ALL NEW SIGNAL POLES IN SIDEWALK AREAS ONLY.
- SIGNAL POLE FOOTER CONSTRUCTION, BACKFILL AND EXCAVATION LIMITS SHALL CONFORM TO STANDARD SHEET 680-01. PAYMENT SHALL BE MADE UNDER ITEM 680.5001 - POLE EXCAVATION AND CONCRETE FOUNDATION.
- TEMPORARY CASING AND DRILLING MUD MAY BE REQUIRED TO EXCAVATE TO THE DESIGN ELEVATION.
- EXPOSED FOUNDATION SURFACES SHALL BE FINISHED IN A WORKMAN LIKE MANNER. REINFORCEMENT BARS SHALL HAVE A MINIMUM OF 3 INCHES OF COVER. FOUNDATIONS SHALL BE AT THE SAME ELEVATION AS EXISTING OR PROPOSED FINISH WALKWAY SURFACES. FOUNDATIONS IN TURF AREAS SHALL BE 2 INCHES ABOVE FINAL GRADE.
- MORTAR CAPS SHALL NOT BE USED.
- BASE PLATES SHALL BE WRAPPED WITH A STAINLESS STEEL SCREEN. SCREEN OPENING SIZE SHALL BE APPROXIMATELY 1/4 INCH. THE SCREEN IS SECURED TO THE BASE PLATE WITH A STAINLESS STEEL BAND. THE SCREEN SHALL BE INSTALLED FROM THE FOUNDATION SURFACE TO JUST BELOW THE TOP OF THE BASE PLATE. THE SCREEN WRAPS THE FULL PERIMETER OF THE BASE PLATE WITH A 3 INCH OVERLAP. THE OVERLAP SHALL FACE AWAY FROM TRAFFIC.
- SIGNAL POLE FOUNDATIONS SHALL CURE A MINIMUM OF 14 DAYS BEFORE THE POLE IS ERECTED AND A MINIMUM OF 28 DAYS BEFORE THE SIGNAL HEAD ASSEMBLIES, SPAN WIRE OR MAST ARMS ARE INSTALLED.

ELECTRICAL CABLE SPLICES

- ALL CABLE SPLICES MADE IN CABLE RUNS TO BE LOCATED BELOW GROUND WILL BE ACCOMPLISHED USING METHOD NO. 2 (TWO COMPONENT ELECTRICAL INSULATING RESIN REJACKETING MATERIALS) AS DESCRIBED IN SECTION 680-3.16 OF THE N.Y.S. STANDARD SPECIFICATIONS.
- EXTRA CONDUCTORS IN SIGNAL CABLE RUNS SHALL NOT BE CUT SHORT BUT WILL BE TAPED BACK ON ITS OWN CABLE FOR FUTURE USE.
- OVERHEAD CABLE SPLICES WILL NOT BE ALLOWED.

CONDUIT AND CONDUIT EXCAVATION

- CAPPED CONDUIT SHALL BE THE SIZE SHOWN ON THE PLANS AND SHALL EXTEND A MINIMUM OF 6 FEET OUTSIDE ALL POLE FOUNDATIONS OR PULLBOXES.
- EXCAVATION FOR THE INSTALLATION OF UNDERGROUND CONDUIT IN PAVEMENT AREAS SHALL BE MADE WITH AN APPROPRIATE WHEEL EXCAVATOR. THE TRENCH WIDTH SHALL BE KEPT TO A MINIMUM AND THE DEPTH SHALL BE IN CONFORMANCE WITH THE N.Y.S. STANDARD SPECIFICATIONS.
- IN PAVEMENT AREAS TRENCH BACKFILL SHALL BE CONTROLLED LOW STRENGTH MATERIAL TO THE BOTTOM OF THE ASPHALT COURSE. IT SHALL CONFORM TO THE N.Y.S. STANDARD SPECIFICATIONS.
- ALL SPARE CONDUITS SHALL CONTAIN A CONTINUOUS POLY LINE TIED OFF IN THE PULL BOX AT EACH END.
- ALL CONDUIT CONNECTIONS SHALL BE WATERTIGHT.
- ALL SIGNAL CONDUITS SHALL BE BONDED WITH A #6 BARE STRANDED COPPER GROUND WIRE.
- ALL COUPLINGS AND BUSHINGS SHALL BE MECHANICALLY THREADED. COMPRESSION OR SLIP TYPE COUPLINGS OR BUSHINGS SHALL NOT BE ALLOWED.
- WHENEVER ITEM 206.03 CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION INVOLVES SAW CUTTING ACROSS THE WIDTH OF THE SIDEWALK, THE AFFECTED SLABS SHALL BE FULLY REMOVED AND REPLACED. THE COST OF THIS WORK SHALL BE INCLUDED IN THIS ITEM.

TRAFFIC SIGNAL ASSEMBLIES

- DESIGN CLEARANCE UNDER ALL STRUCTURES SHALL HAVE A MINIMUM CLEARANCE OF 17 FEET.

GROUNDING

- ALL SIGNAL POLES OR PEDESTRIAN STATIONS SHALL BE GROUNDED IN CONFORMANCE WITH THE DETAILS SHOWN ON STANDARD SHEET 680-04, WITH THE EXCEPTION OF THE DETAIL "GROUND ROD INSTALLATION", THE "GROUND ROD INSTALLATION" DETAIL SHALL NOT BE INSTALLED IN THIS CONTRACT.
- USE AT LEAST A 15 INCH DIAMETER REINFORCED CONCRETE PULLBOX FOR GROUNDING ROD INSTALLATION.
- CLAY TILE PIPE DETAILED ON STANDARD SHEET 680-04 SHALL NOT BE USED.
- GROUND WIRE FOR CABINETS OR POLES SHALL BE INSTALLED IN ITS OWN 1 INCH P.V.C. PLASTIC CONDUIT AND RUN TO THE NEAREST GROUND ROD LOCATION AS SHOWN ON STANDARD SHEET 680-04.
- CONSTRUCTION SHALL COMPLY WITH THE NATIONAL ELECTRIC CODE.
- ALL GROUND TESTING SHALL BE COMPLETED AND ACCEPTED PRIOR TO SIGNAL START UP.

OVERHEAD SIGN MOUNTING

- OVERHEAD SIGNS:
ALL ADDITIONAL MATERIALS REQUIRED FOR THE INSTALLATION OF OVERHEAD SIGN ASSEMBLIES SHALL BE INCLUDED IN THE PRICE BID FOR THE ITEM.
- SIGNS SHALL BE MOUNTED SO THAT THEY ARE CENTERED AND FACING THE LANES THAT THEY REGULATE, SIGNAL HEADS SHALL BE LOCATED TO ALLOW FOR THIS CENTERED PLACEMENT OF THE SIGNS.
- ANY LATERAL ADJUSTMENT TO PROPERLY ALIGN A STRUCTURE ON THE MAST ARM SHALL BE INCLUDED IN THE ITEMS ASSOCIATED WITH THAT STRUCTURE.

PULLBOXES

- UNLESS OTHERWISE SHOWN. ALL PULLBOXES SHALL BE INSTALLED OUTSIDE OF THE PAVEMENT OR SHOULDER AREAS.
- THE FINISHED GROUND SURFACE ON A BACK SLOPE IN THE VICINITY OF THE PULLBOX SHALL BE ADJUSTED SO THAT NO FILL SHALL SPILL ON THE TOP OF THE BOX AND THE MAXIMUM DISTANCE FROM THE TOP OF THE BOX TO THE FINISHED GROUND AT THE BOX SHALL NOT EXCEED 4 INCHES. ALL MATERIALS AND LABOR NECESSARY TO COMPLETE THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE PULLBOX.
- ALL PULLBOXES INSTALLED IN THE SIDEWALK SHALL BE FLUSH WITH THE WALK.
- MAXIMUM DEPTH TO CONDUIT ENTRANCE SHALL BE 3 FEET.

WORK AFFECTING SIGNALS AND COORDINATION WITH OWNER

- THE CONTRACTOR SHALL NOTIFY THE CITY OF PLATTSBURGH IN WRITING AT LEAST TEN (10) WORKING DAYS IN ADVANCE OF ANY OF HIS OPERATIONS THAT WILL AFFECT THE EXISTING SIGNAL, SUCH AS PAVEMENT REMOVAL OR NEW SIGNAL ITEM INSTALLATIONS. THE TRAFFIC DEPARTMENT SHALL PERFORM ALL NECESSARY WORK REQUIRED IN THE EXISTING CONTROLLER CABINET, HOWEVER, AT THE OVERHEAD REBUILD SITES THE CONTRACTOR WILL BE REQUIRED TO MAKE THE CONNECTIONS FOR THE OUTPUTS TO THE HEADS ON THE TERMINAL STRIP IN THE CABINET.
- FINAL SIGNAL HEAD LOCATIONS SHALL BE FIELD REVIEWED AND APPROVED BY THE CITY OF PLATTSBURGH.
- ALL WIRING SHALL BE COMPLETED IN A NEAT AND WORKMAN LIKE MANNER.

ELECTRIC SERVICE ENTRANCE

- THE UNDERGROUND SERVICE DETAIL ON STANDARD SHEET 680-05 SHALL BE MODIFIED SO THAT THE NORMAL 1 NPS CONDUIT FOR UNDERGROUND SERVICE CONDUCTORS SHALL BE 2 NPS GALVANIZED STEEL HEAVY WALL CONDUIT.
- ALL SERVICE ENTRANCES SHALL HAVE A METER, METER PAN, AND AN ELECTRICAL DISCONNECT/GENERATOR TRANSFER SWITCH.
- METER CABINET ITEM 680.95533211 SPECIFICATION SHALL BE MODIFIED TO MEET PMLD REQUIREMENTS.

CONDUITS

- ALL SIGNAL CONDUITS SHALL BE BONDED WITH A #6 BARE STRANDED COPPER GROUND WIRE.
- ALL COUPLINGS AND BUSHINGS SHALL BE MECHANICALLY THREADED. COMPRESSION OR SLIP TYPE COUPLINGS OR BUSHINGS SHALL NOT BE ALLOWED.
- WHENEVER ITEM 206.03 CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION INVOLVES SAW CUTTING ACROSS THE WIDTH OF THE SIDEWALK, THE AFFECTED SLABS SHALL BE FULLY REMOVED AND REPLACED. THE COST OF THIS WORK SHALL BE INCLUDED IN THIS ITEM.

PEDESTRIAN SIGNALS

- ALL PEDESTRIAN PUSH BUTTONS SHALL CONFORM TO THE AMERICANS WITH DISABILITIES ACT (ADA) REQUIREMENTS.
- ALL PEDESTRIAN PUSH BUTTONS SHALL BE LOCATED PARALLEL WITH THE APPROPRIATE CROSSWALK. ALSO, ALL INDICATIONS SHALL BE INSTALLED AWAY FROM TURNING VEHICULAR TRAFFIC UNLESS OTHERWISE SHOWN ON THE PLAN SHEET.
- ALL PEDESTRIAN SIGNS SHALL BE 9 INCHES X 15 INCHES (R10-3e) REGARDLESS OF THE SIZE OF POLE THEY ARE INSTALLED ON AND SHALL BE ALUMINUM MATERIAL. SIGNS SHALL BE MOUNTED ON YELLOW BRACKET MATCHING COLOR OF PUSH BUTTON. THE COST OF FURNISHING AND INSTALLING THESE SIGNS SHALL BE INCLUDED IN THE BID PRICE FOR ITEM 680.81330010 - AUDIBLE PEDESTRIAN SIGNAL.
- ON STANDARD SHEET NO. 680-10, ONLY TOP MOUNT POST INSTALLATION SHALL BE ALLOWED.
- ON STANDARD SHEET NO. 680-10, THE PEDESTRIAN INDICATIONS SHALL BE BANDED TO THE POLE AND HAVE ONE BOLT THROUGH THE TOP BRACKET.
- PEDESTRIAN HEAD HOUSINGS SHALL BE FEDERAL YELLOW IN COLOR.
- PEDESTRIAN SIGNAL HEADS SHALL BE ALUMINUM AND MANUFACTURED BY PEEK TRAFFIC CORPORATION.
- STANDARD SPECIFICATION 680.67XX IS MODIFIED ALLOWING ONLY ALUMINUM TRAFFIC SIGNAL POLES UNLESS OTHERWISE SHOWN ON THE PLANS.

UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL UTILITY WORK WITH THE RESPECTIVE OWNERS DURING CONSTRUCTION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.
- CONTRACTOR IS TO CONTACT DIG SAFELY NEW YORK AT 811 FOR MARK OUT OF ALL UNDERGROUND SIGNAL CONDUIT AND LOOP WIRE PRIOR TO ANY DIGGING. ANY BROKEN SIGNAL WIRE CAN NOT BE SPLICED. A NEW RUN OF WIRE FROM THE POLE TO THE FURTHEST END MUST BE REPLACED.

EMERGENCY REPAIR

- THE CONTRACTOR SHALL PROVIDE THE LOCAL POLICE THE NAME, ADDRESS, AND TELEPHONE NUMBER OF A PERSON OR PERSONS AUTHORIZED OUTSIDE THE NORMAL CONTRACT WORKING HOURS TO SECURE AND USE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS TO MAKE SAFE THE ENTIRE AREA OF THE CONTRACT. DUPLICATE COPIES OF SUCH NOTICES SHALL BE FILED WITH THE EIC, TRAFFIC SIGNAL MAINTENANCE, AND THE CITY OF PLATTSBURGH.

AUDIBLE WIRING

- THE MANUFACTURER'S SUGGESTED CABLES (OR EQUIVALENTS) SHALL BE USED WITH AUDIBLE PEDESTRIAN SIGNAL (APS) UNITS. THE QUANTITY OF CABLE REQUIRED AT THIS LOCATION SHALL BE DETERMINED BY THE CONTRACTOR IN ACCORDANCE WITH THE APS MANUFACTURER'S INSTALLATION DETAILS AND PROCEDURES.
- REFER TO THE MANUFACTURER'S INSTALLATION AND PROCEDURE MANUALS FOR DETAILS.
- THE COST TO FURNISH AND INSTALL ALL WIRING BETWEEN THE AUDIBLE PEDESTRIAN SIGNAL (APS) UNITS FOR ITEM 680.81330010 AND THEIR RESPECTIVE CONTROL UNITS MOUNTED IN THE PED. HEADS SHALL BE INCLUDED UNDER PROVISIONS OF ITEM 680.81330010.

APS

- CONTRACTOR SHALL ORDER APS BUTTONS FROM MANUFACTURER WITH STREET NAME MESSAGES PRE-INSTALLED. CONTRACTOR SHALL PROVIDE COPIES OF THE ASSOCIATED ELECTRONIC WAVE FILES TO THE CITY OF PLATTSBURGH.
- APS PUSH BUTTONS SHALL BE POLARA ENGINEERING BDL3 BULLDOG III SERIES VANDAL RESISTANT ADA PUSH BUTTON.

CONTROLLER

- CITY OF PLATTSBURGH TO PROGRAM THE CONTROLLERS, EXCEPT AT OAK/BRINKERHOFF. CONTRACTOR SHALL COORDINATE INSTALLATION WITH THE CITY.
- SUPPLIER SHALL PROVIDE INITIAL PROGRAMMING AND PROVIDE EIGHT (8) HOURS OF TRAINING FOR PMLD PERSONNEL. COST TO BE INCLUDED IN PRICE BID FOR ITEM 680.80149305.
- CABINET AND CONTROLLER AT OAK & BRINKERHOFF SHALL BE NORTHEAST SIGNAL KA8P6-39DOE POLE MOUNTED CABINET WITH PEEK 3000E NEMA TS2 TYPE 2 CONTROLLER WITH DOUBLE DIAMOND CONFLICT MONITOR.
- UPON REMOVAL OF EXISTING CABINET, CONTROLLER, AND METER AT OAK AND BRINKERHOFF, CONTRACTOR SHALL RETURN OLD EQUIPMENT TO THE CITY OF PLATTSBURGH.

SALVAGE ITEMS

- EXISTING PEDESTRIAN POLES, CONTROLLERS, CABINETS, PUSH BUTTONS, AND SIGN PANELS REMOVED BY CONTRACTOR SHALL BE RETAINED BY THE CITY, FOR PICKUP BY THE CITY OF PLATTSBURGH DPW.

VEHICLE DETECTION

- CITY OF PLATTSBURGH PMLD SHALL PROVIDE AND INSTALL MICROWAVE DETECTION AT OAK/BRINKERHOFF. CONTRACTOR TO COORDINATE INSTALLATION WITH THE CITY.

* DEDUCT ALTERNATE #1



C&S Engineers, Inc.
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CITY OF PLATTSBURGH
CLINTON COUNTY, NEW YORK

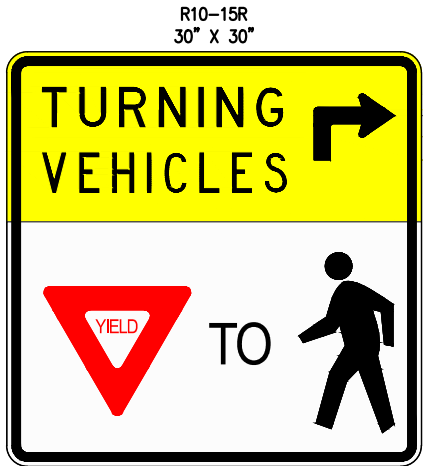
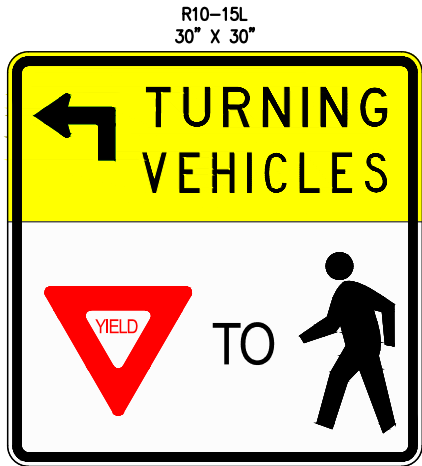
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TRAFFIC SIGNAL
NOTES

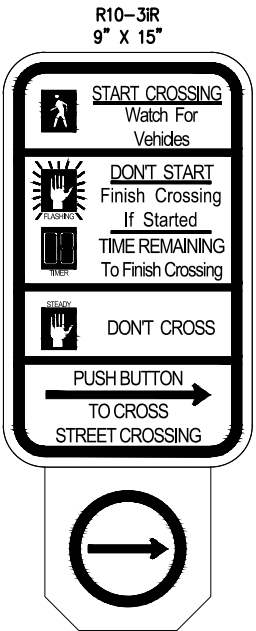
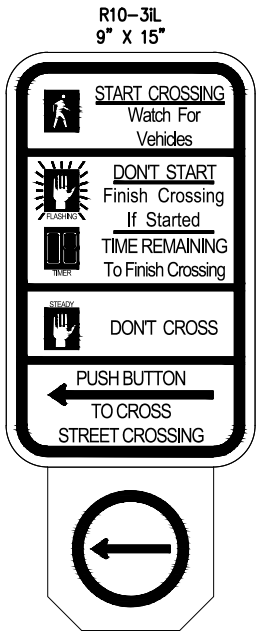
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SHEET 15 OF 20

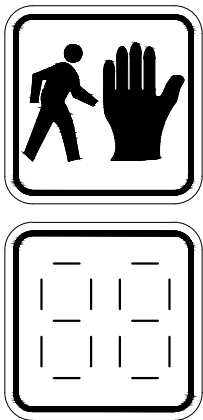
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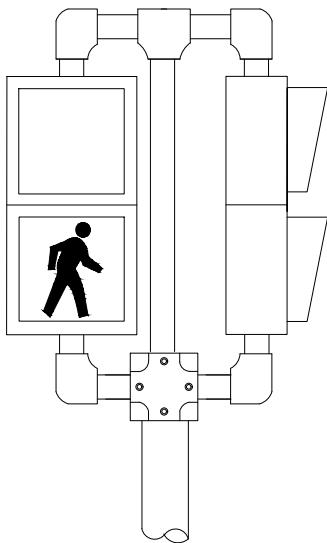
POLE, MAST ARM, OR SPAN WIRE MOUNTED YIELD SIGN
NOT TO SCALE



APS PUSH BUTTON AND SIGN
NOT TO SCALE

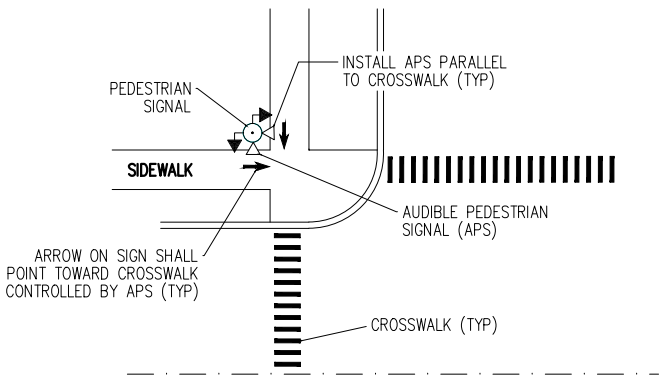


HAND/MAN MODULE AND COUNT-DOWN TIMER
NOT TO SCALE

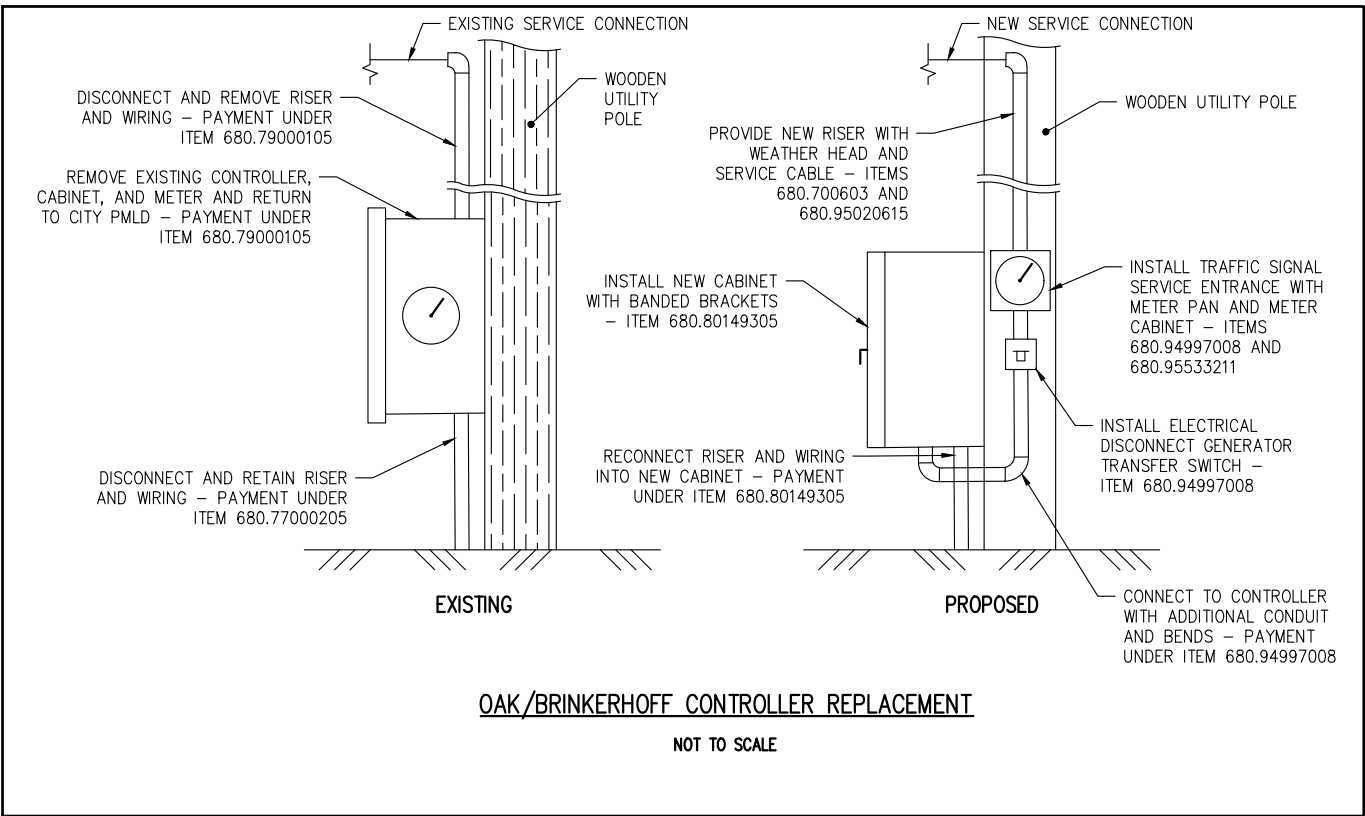


PEDESTRIAN SIGNAL HEAD MOUNTING DETAIL
NOT TO SCALE

- NOTE:
1. POST TOP MOUNT PEDESTRIAN SIGNAL HEADS IN THIS CONTRACT (BI-DIRECTIONAL) SHALL BE INSTALLED WITH A REINFORCING PLATE AND TUBING AS SHOWN IN THE DETAIL.



AUDIBLE PEDESTRIAN SIGNAL (APS)
INSTALLATION SCHEMATIC
NOT TO SCALE



OAK/BRINKERHOFF CONTROLLER REPLACEMENT
NOT TO SCALE

DEDUCT ALTERNATE #1



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TRAFFIC SIGNAL
DETAILS

TSN-2

SHEET 16 OF 20

Dec 21, 2020 - 12:26pm
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TABLE OF TRAFFIC SIGNAL QUANTITIES								
	ITEM NUMBER	DESCRIPTION	UNIT	BASE BID				
				CORNELIA ST AT BROAD ST	CORNELIA ST AT PROSPECT AVE	CORNELIA ST AT BEEKMAN ST	DEDUCT ALTERNATE #1	DEDUCT ALTERNATE #2
							OAK ST AT BRINKERHOFF ST	CORNELIA ST AT N CATHERINE ST
1	208.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	LF	45	50	35	60	35
2	619.1613	MAINTAIN TRAFFIC SIGNAL EQUIPMENT (REQUIREMENT C)	INTM	1	1	1	1	1
3	635.0103	CLEANING AND PREPARATION OF PAVEMENT SURFACES - LINES	LF	760	1225	2180	2680	890
4	645.85	POLE MOUNTED SIGN SUPPORT SYSTEM (BAND MOUNTED)	EA		1	1		
5	647.31	RELOCATE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE 1 (UNDER 30 SQUARE FEET)	EA		1			1
6	680.5001	POLE EXCAVATION AND CONCRETE FOUNDATION	CY	2.0	1.6	1.6	1.2	1.6
7	680.510501	PULLBOX - RECTANGULAR, 26" X 18", REINFORCED CONCRETE	EA	1	3		5	5
8	680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	LF	45	50	35	60	35
9	680.520108	CONDUIT, METAL STEEL, ZINC COATED, 3 NPS	LF	175	310	120	330	320
10	680.53	CONDUIT JACKING OR BORING	LF	175	310	120	330	320
11	680.6708	TRAFFIC SIGNAL POLE POST TOP MOUNT, 8 FEET MOUNTING HEIGHT	EA	5	4	4	3	4
11A	680.700603	RISER ASSEMBLY, 1" DIAMETER	EA				1	
12	680.730714	SIGNAL CABLE 7 CONDUCTORS, 14 AWG	LF	1000	1140	900	1000	880
13	680.77000105	MODIFY TRAFFIC SIGNAL INSTALLATION, LOCATION 1	ELOC	1			1	
14	680.77000205	MODIFY TRAFFIC SIGNAL INSTALLATION, LOCATION 2	ELOC					
15	680.78010005	ALTER PULLBOX FOR CONDUITS	EA	4	1	4		
16	680.79000105	REMOVE TRAFFIC SIGNAL INSTALLATION, LOCATION 1	ELOC				1	
16A	680.79000205	REMOVE TRAFFIC SIGNAL INSTALLATION, LOCATION 2	ELOC	1				
17	680.80149305	NEMA TS 2 SOLID STATE MENU DRIVEN ACTUATED TRAFFIC SIGNAL CONTROLLER AND CABINET	EA				1	
18	680.813105	PEDESTRIAN SIGNAL MODULE - 12 INCH, BI-MODAL HAND/MAN SYMBOLS, LED	EA	5	8	8	8	8
19	680.813103	PEDESTRIAN SIGNAL SECTION - TYPE 1, 12 INCH	EA	10	16	16	16	16
20	680.81330010	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POLE	EA	8	8	8	8	8
21	680.8141	PEDESTRIAN SIGNAL BRACKET MOUNT ASSEMBLY	EA				1	
22	680.8142	PEDESTRIAN SIGNAL POST TOP MOUNT ASSEMBLY	EA	5	4	4	3	4
23	680.81500010	PEDESTRIAN COUNTDOWN TIMER MODULE	EA	5	8	8	8	8
24	680.8204	OVERHEAD SIGN ASSEMBLY, TYPE D	EA	3	1	1	3	1
25	680.8207	OVERHEAD SIGN ASSEMBLY, TYPE G	EA	1				
26	680.94000008	TRAFFIC SIGNAL SERVICE ENTRANCE	EA				1	
27	680.94997008	FURNISH AND INSTALL ELECTRICAL DISCONNECT GENERATOR TRANSFER SWITCH	EA				1	
28	680.95020615	SERVICE CABLE , 2 CONDUCTOR NO. 06 AWG	LF				30	
29	680.95533211	METER CABINET	EA				1	
30	685.11120009	EPOXY REFLECTORIZED PAVEMENT STRIPES 20 MILS - HAND WORK	LF	1400	1110	1220	1610	1160



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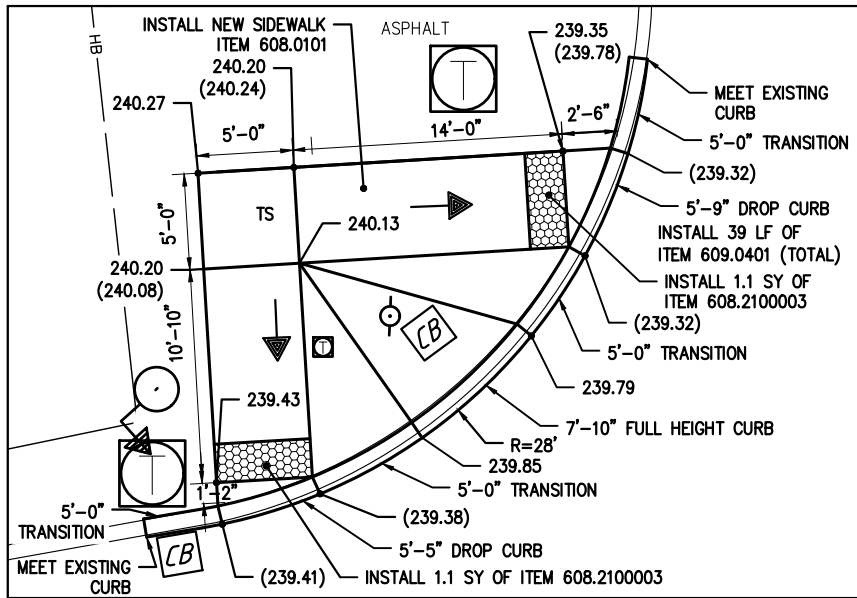
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TRAFFIC SIGNAL
QUANTITIES

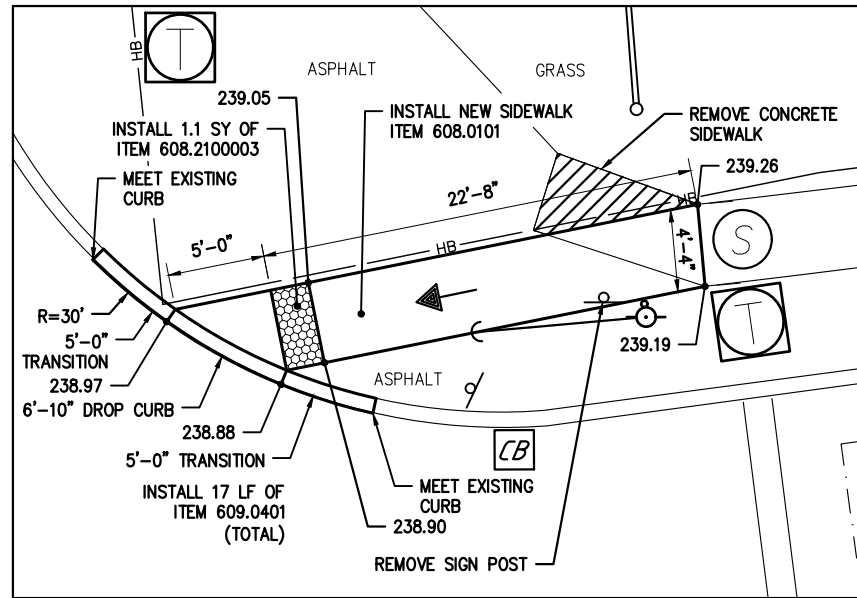
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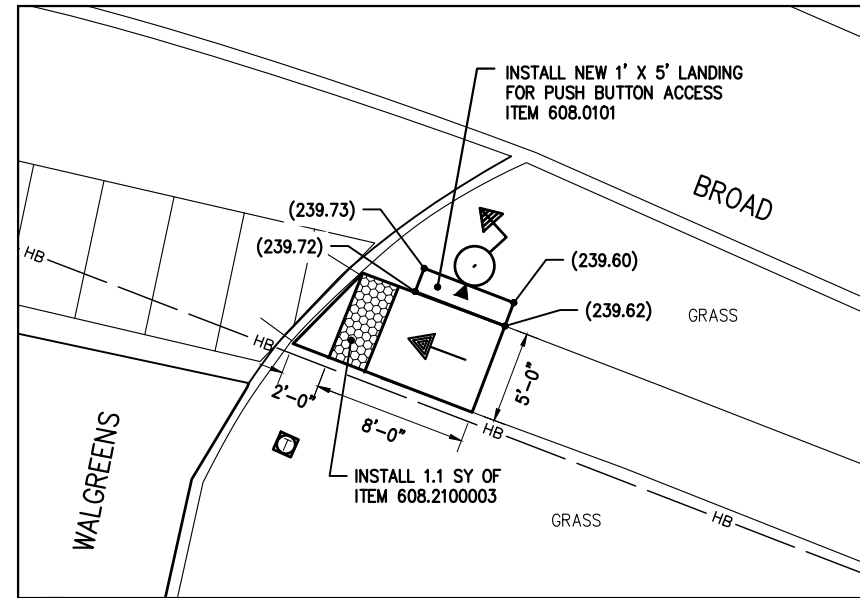
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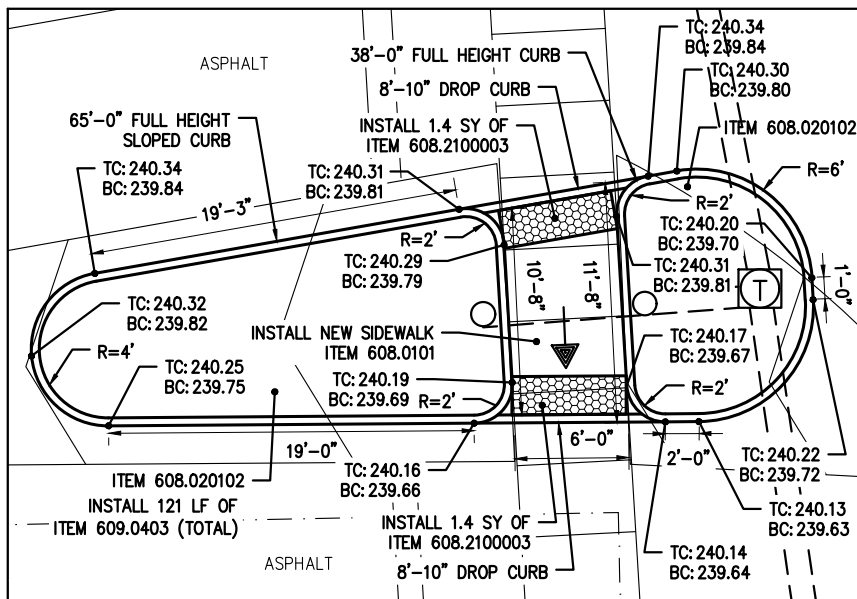
CORNELIA STREET AND BROAD STREET
NORTHWEST QUADRANT



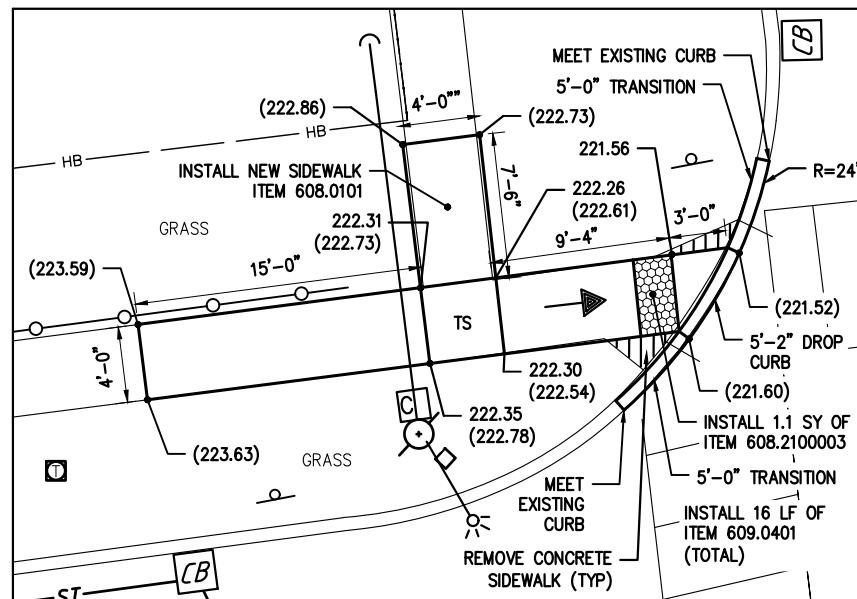
CORNELIA STREET AND BROAD STREET
NORTHEAST QUADRANT



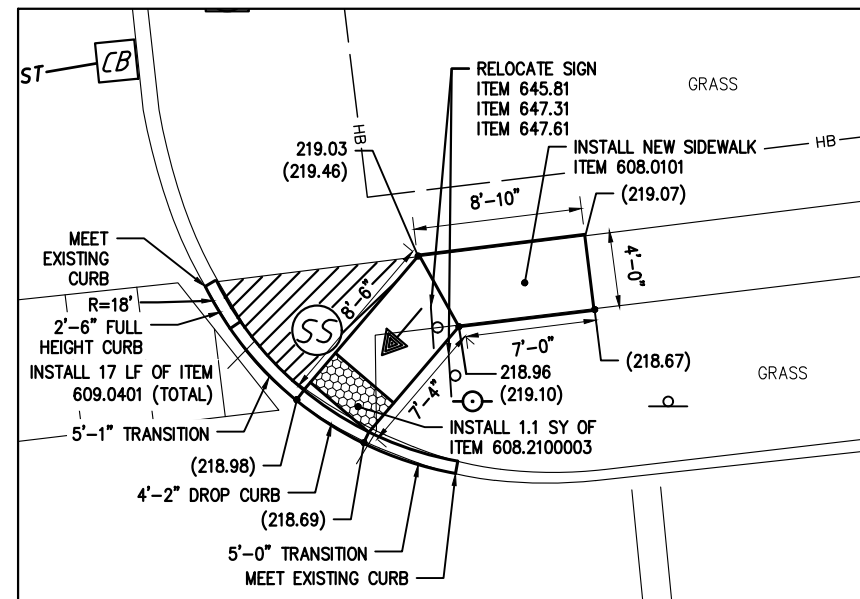
CORNELIA STREET AND BROAD STREET
SOUTHEAST QUADRANT



CORNELIA STREET AND BROAD STREET
RAISED ISLAND



CORNELIA STREET AND PROSPECT AVENUE
NORTHWEST QUADRANT



CORNELIA STREET AND PROSPECT AVENUE
NORTHEAST QUADRANT

NOTES:

- THE DETAILS AND DIMENSIONS PROVIDED ARE APPROXIMATE TO ACHIEVE ADA COMPLIANT RAMPS. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES TO ENSURE COMPLIANCE WITH ADA REQUIREMENTS AND NYSDOT STANDARD SHEET 608-01 (SHEETS 1-12)
- IF LAYOUTS REQUIRE ADJUSTMENT DUE TO FIELD CONDITIONS, NOTIFY ENGINEER IMMEDIATELY.
- SIDEWALK REMOVALS SHALL END AT AN EXISTING JOINT.
- REFER TO NYSDOT STANDARD SHEET 608-01 (SHEETS 1-12) FOR ADDITIONAL CURB RAMP NOTES AND DETAILS.
- ARROWS ON RAMPS REPRESENT DOWN GRADE DIRECTION. THE RUNNING SLOPE OF A CURB RAMP SHALL BE 7.5% MAXIMUM. SEE NOTES ON STANDARD SHEET 608-01 (SHEET 1 OF 12).
- REFER TO DWG NO. MSD-01 FOR SIDEWALK INSTALLATION SECTION.
- SYMBOLS (SIGNAL POLES, PULLBOXES, MANHOLES, VALVES, MONUMENTS, DRAINAGE STRUCTURES, ETC.) MAY BE EXAGGERATED FOR CLARITY.

LEGEND:

- DETECTABLE WARNING UNIT
(ITEM 608.21000003)
- CONCRETE SIDEWALK AREA
(ITEM 608.0101)
- REMOVE EXISTING SIDEWALK
- TS
TURNING SPACE. SLOPE NOT TO
EXCEED 1.5% IN ANY DIRECTION.
- XXX.XX
PROPOSED ELEVATION
- (XXX.XX)
EXISTING ELEVATION
- RAMPS



LOCAL PEDESTRIAN SAFETY
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CURB RAMP
DETAILS

CRD-1

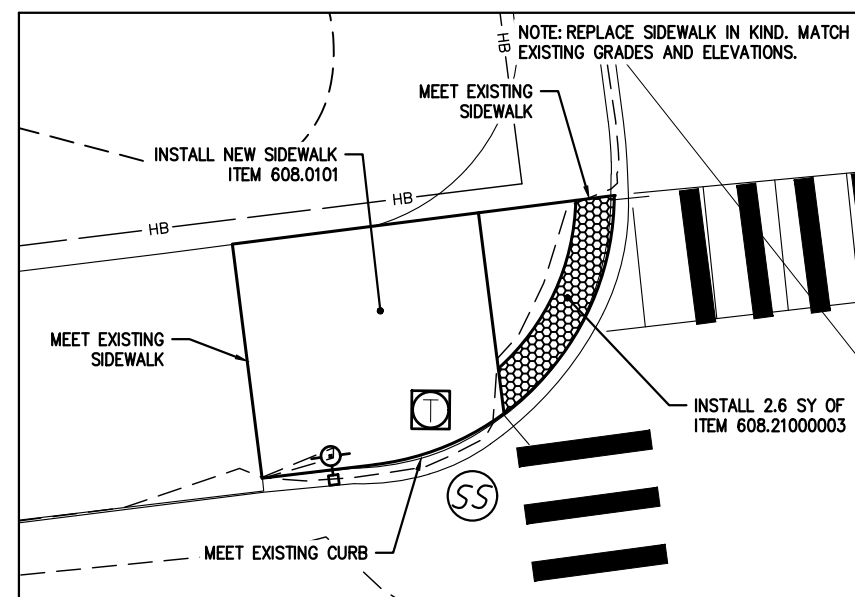
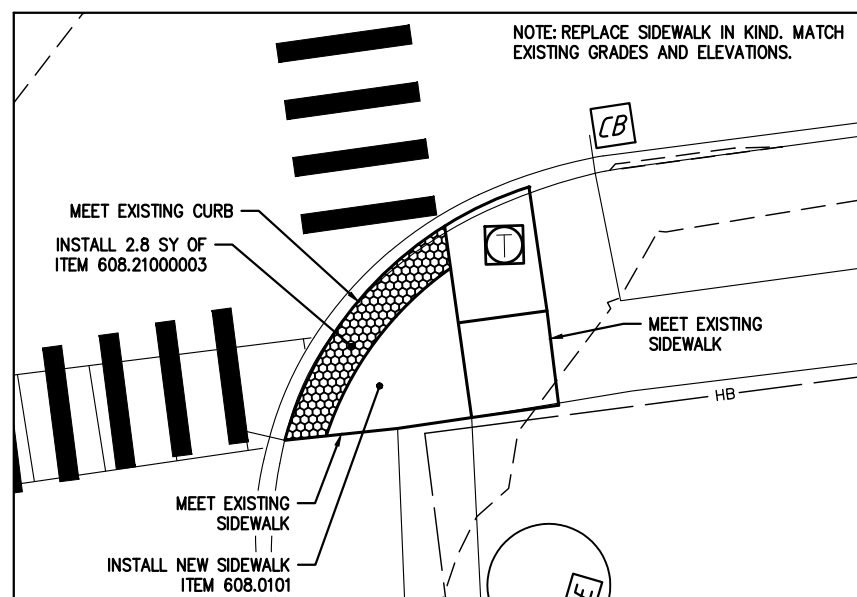
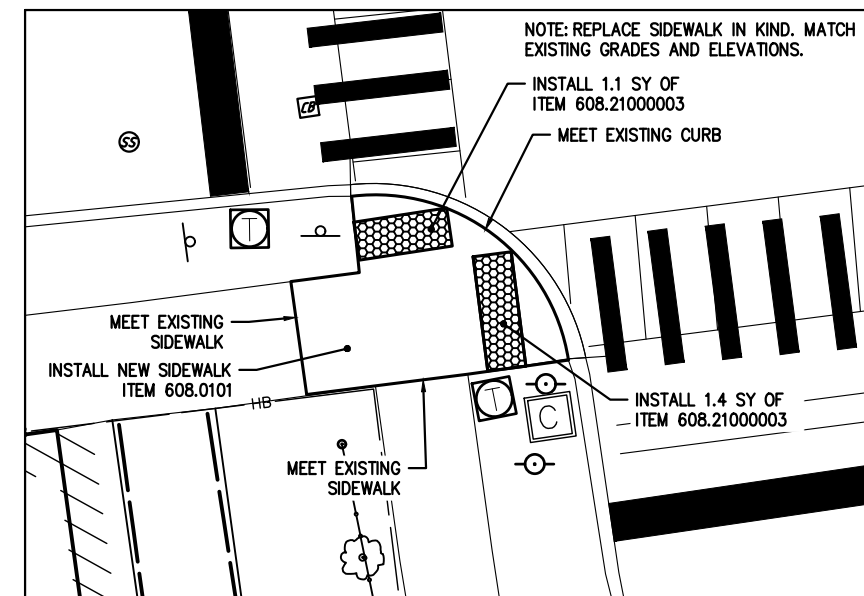
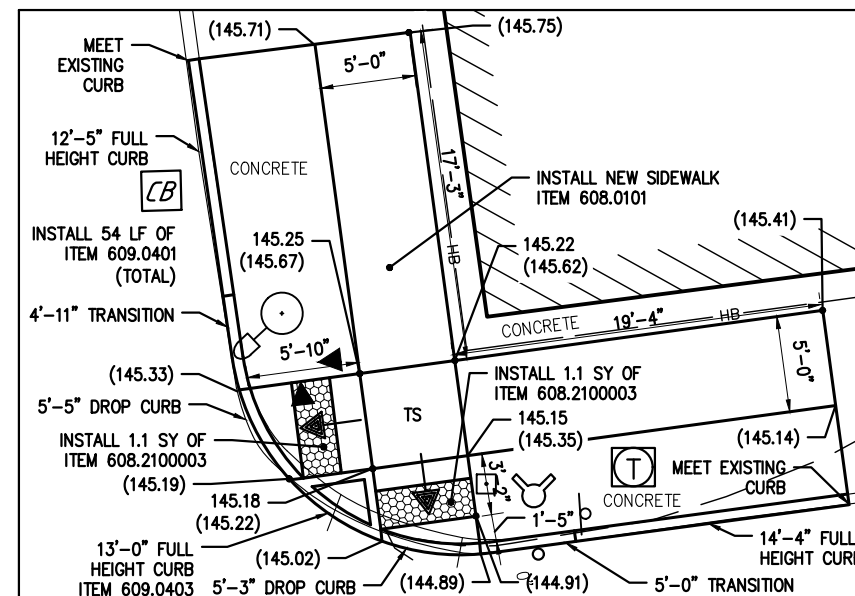
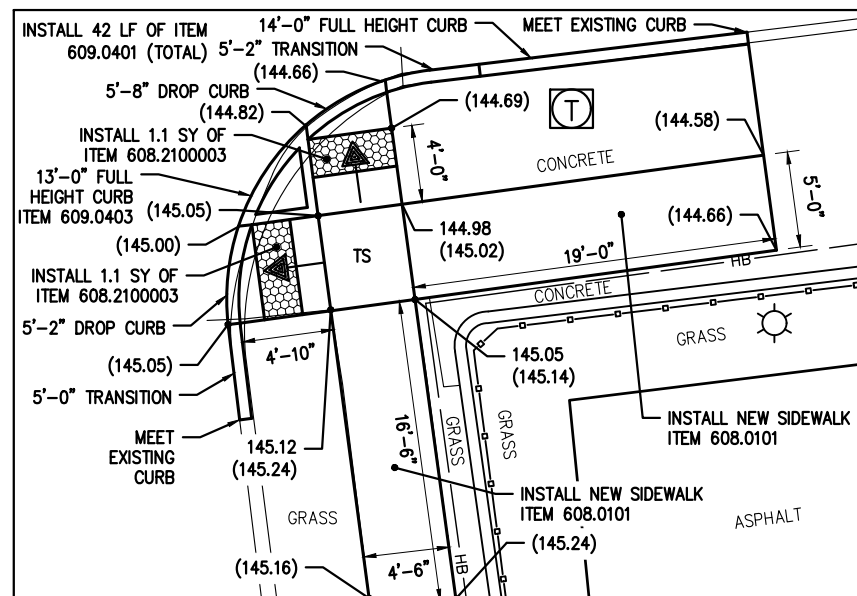
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NOTES:

1. THE DETAILS AND DIMENSIONS PROVIDED ARE APPROXIMATE TO ACHIEVE ADA COMPLIANT RAMPS. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES TO ENSURE COMPLIANCE WITH ADA REQUIREMENTS AND NYSDOT STANDARD SHEET 608-01 (SHEETS 1-12)
2. IF LAYOUTS REQUIRE ADJUSTMENT DUE TO FIELD CONDITIONS, NOTIFY ENGINEER IMMEDIATELY.
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4. REFER TO NYSDOT STANDARD SHEET 608-01 (SHEETS 1-12) FOR ADDITIONAL CURB RAMP NOTES AND DETAILS.
5. ARROWS ON RAMPS REPRESENT DOWN GRADE DIRECTION. THE RUNNING SLOPE OF A CURB RAMP SHALL BE 7.5% MAXIMUM. SEE NOTES ON STANDARD SHEET 608-01 (SHEET 1 OF 12).
6. REFER TO DWG NO. MSD-01 FOR SIDEWALK INSTALLATION SECTION.
7. SYMBOLS (SIGNAL POLES, PULLBOXES, MANHOLES, VALVES, MONUMENTS, DRAINAGE STRUCTURES, ETC.) MAY BE EXAGGERATED FOR CLARITY.

LEGEND:

- | | |
|---|--|
|  | DETECTABLE WARNING UNIT
(ITEM 608.21000003) |
|  | CONCRETE SIDEWALK AREA
(ITEM 608.0101) |
|  | REMOVE EXISTING SIDEWALK |
| TS | TURNING SPACE. SLOPE NOT TO
EXCEED 1.5% IN ANY DIRECTION. |
| XXX.XX | PROPOSED ELEVATION |
| (XXX.XX) | EXISTING ELEVATION |
|  | RAMP GRADE |



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