

# Parking Replacement Options

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DISCOVER  
Plattsburgh

# WARNING: WORKING DRAFT DESIGNS

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**It is important to note all of the following images and parking plans will need final approvals and final design / engineering work.**

# Purpose

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PPAC's role is to behave as an advisory board that makes recommendations to the Common Council.

This will be a chance to go over several parking replacement options and as a PPAC provide recommendations and feedback on each one.

Our comments will then be distributed to the Common Council for their review and to take into consideration which parking replacement options to pursue.

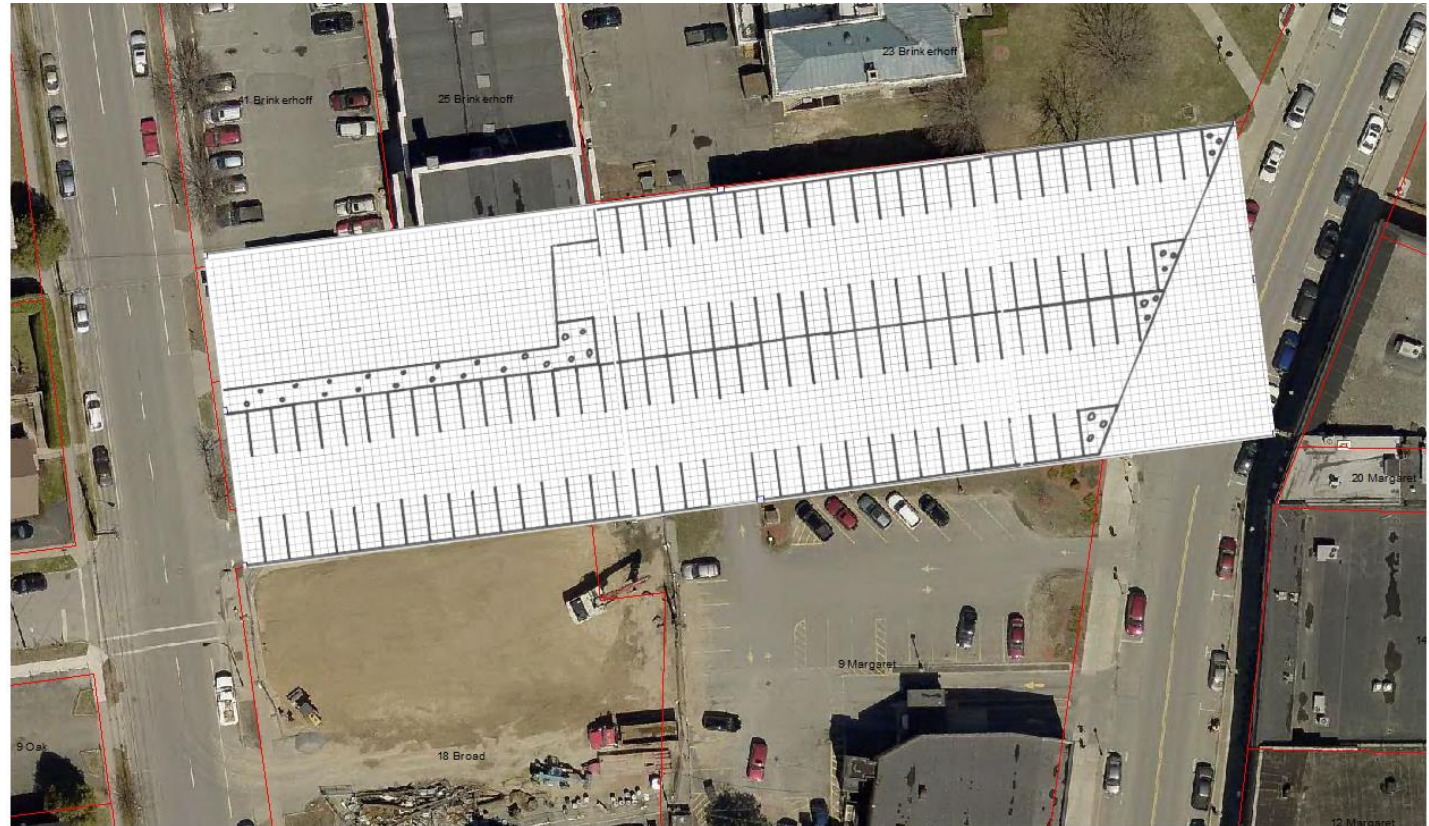
# Arnie Pavone Memorial Parking Plaza

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**+115 spaces**

24 feet driving lanes  
which could allow 2 way  
traffic.

Exit built in for  
Community Bank  
Customers.



# Durkee Street Development

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## **+50 Spaces**

Based on recent revisions to their plans for the Durkee Street development, Prime has included at least 50 public parking spaces on-site. This is an increase from the 30 spaces they had previously reserved. These changes will be discussed in more detail during the Common Council meeting on June 13<sup>th</sup>.



# Clinton County Parking Reconfiguration

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## +60 Spots

The County is looking to reconfigure their existing parking lots (Main Campus / Oak Street) which is expected to increase total parking capacity by 70 spots. The County would convert their 60 spot “visitor parking lot” into publicly available parking.

This parking will reduce the demand on the surrounding streets and lots from County employees / visitors / jurors. The parking will also benefit the City for special events and evening parking supply.

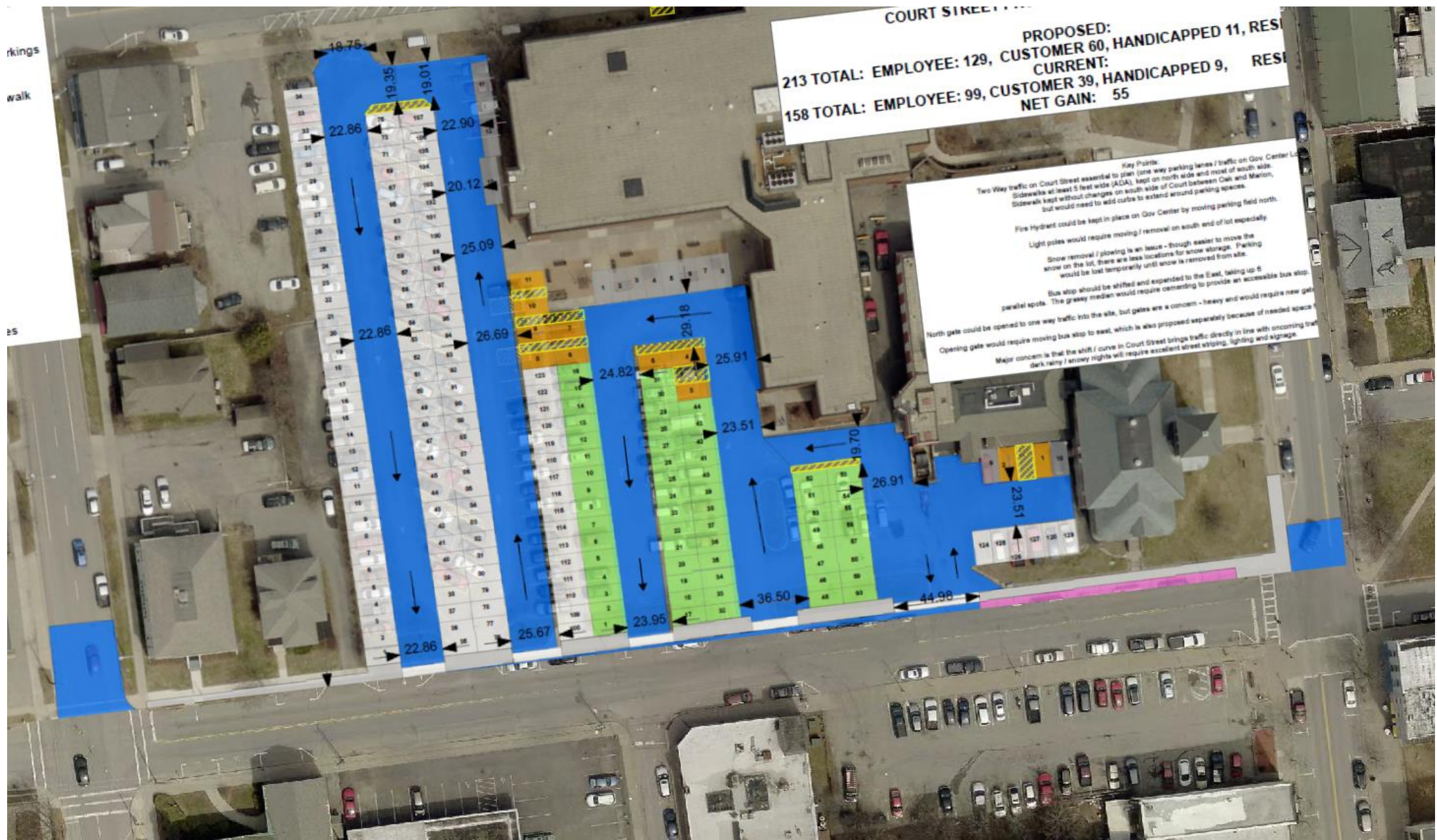
Cost estimate for this option: \$217,670

Cost per newly constructed space: \$7,773

Cost per new publicly available space: \$3,627

rkings  
walk

25



COURT STREET

**PROPOSED:**  
 213 TOTAL: EMPLOYEE: 129, CUSTOMER 60, HANDICAPPED 11, RES

**CURRENT:**  
 158 TOTAL: EMPLOYEE: 99, CUSTOMER 39, HANDICAPPED 9, RES

**NET GAIN: 55**

**Key Points:**

- Two Way traffic on Court Street essential to plan (one way parking lanes / traffic on Gov Center L)
- Sidewalks at least 5 feet wide (ADA), kept on north side and most of south side.
- Sidewalk kept without changes on south side of Court between Oak and Marion, but would need to add curbs to extend around parking spaces.
- Fire Hydrant could be kept in place on Gov Center by moving parking field north.
- Light poles would require moving / removal on south end of lot especially.
- Snow removal / plowing is an issue - though easier to move the snow on the lot, there are less locations for snow storage. Parking would be lost temporarily until snow is removed from site.
- Bus stop should be shifted and expanded to the East, taking up 8 parallel spots. The grassy median would require cementing to provide an accessible bus stop.
- North gate could be opened to one way traffic into the site, but gates are a concern - heavy and would require new gate.
- Opening gate would require moving bus stop to east, which is also proposed separately because of needed space.
- Major concern is that the shift / curve in Court Street brings traffic directly in line with incoming traffic.
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# Durkee Street – On Street (One Way)

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## **+43 Spaces**

The May 23<sup>rd</sup> Durkee Street preliminary site plan presented by Prime Companies showed a net increase of 43 on-street spaces on Durkee Street.

This increase results from the street becoming one way and Prime Companies setting their development further back from the road to allow for additional angled parking to be built.

# Bridge Street – On Street

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## **+6 Spaces**

The May 23<sup>rd</sup> Durkee Street Preliminary Site Plan presented by Prime Companies showed and net increase of 6 spaces on Bridge Street. This is allowed because Prime Companies is setting their building farther back from the road which allows the parking to be built into their site.

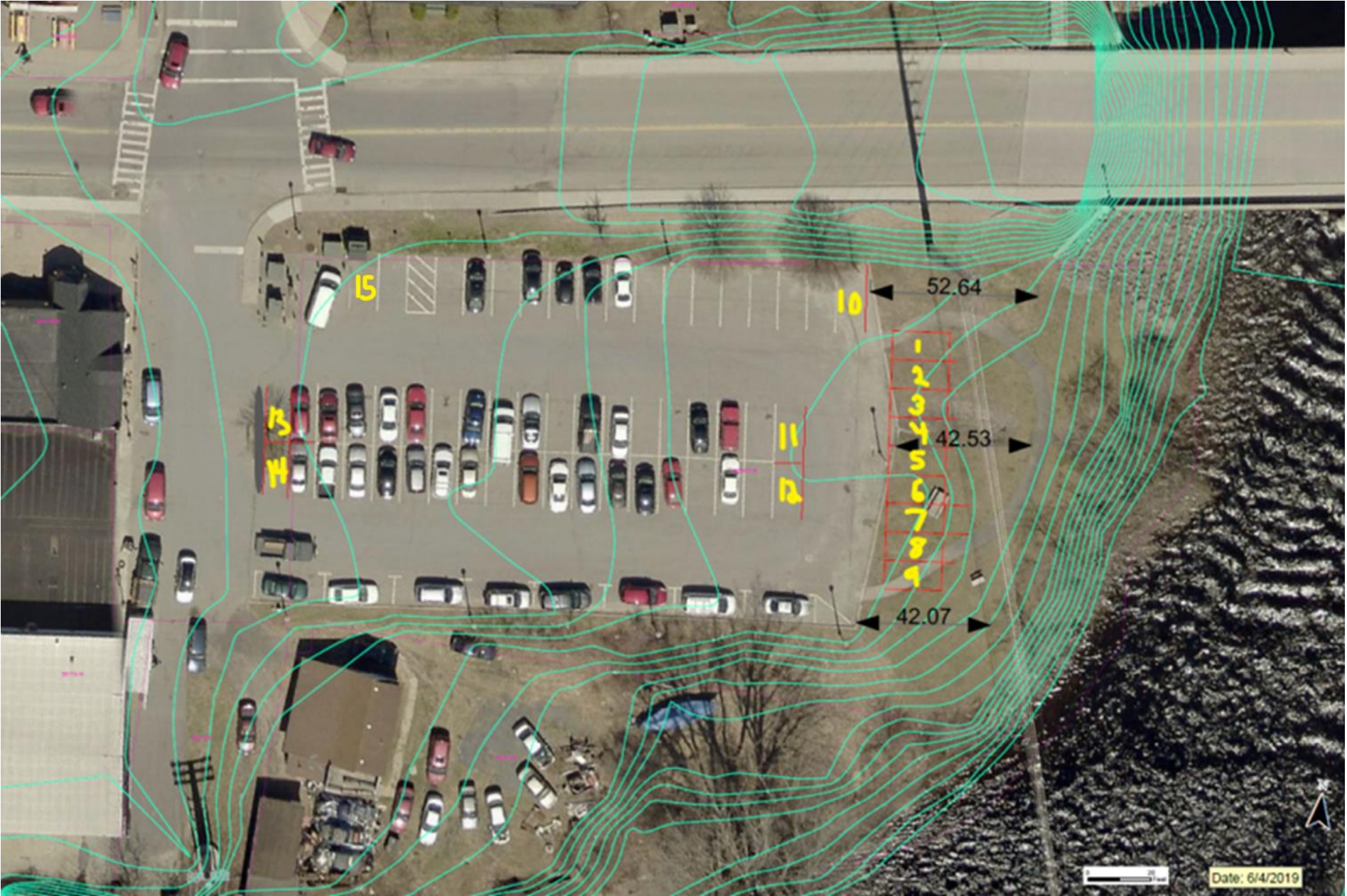
# Broad Street Lot Expansion

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## **+15 Spaces**

Expanding the Broad Street Lot includes expanding the lot by 20 feet towards the Saranac River which will allow for the creation of 9 new parallel parking spaces. The elimination of the small island and restriping allows for the creation 6 new spaces.

Broad Street Lot would increase from 59 spaces to 74 spaces.



Date: 6/4/2019

# Durkee Street – On Street (Two Way)

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## **+27 Spaces**

The May 23rd Prime proposal showed Durkee Street being one way to increase parking capacity. Feedback from the PPAC regarding the possibility of keeping Durkee a two-way street will be of benefit to the Common Council.

If Durkee Street remained two-way it would be possible to have parallel parking on both sides of the road. However, this would complicate the development's connection to the proposed Arts Park and reduce the number of new spaces that could be built on Durkee Street.

The inclusion of parallel parking on both sides of Durkee and maintaining the street as two way would permit the addition of 16 new parking spaces, compared to the 16 additional new spaces that could be included by making Durkee one-way.

# Court Street Reconfiguration Option A

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## +34 Spaces

With this option, the section of Court Street between Margaret Street and Oak Street would behave much more like a parking lot than a street.

Traffic calming measures could be implemented

- Reduced Speed Limit
- Seasonal Speed Bumps
- Stop Signs

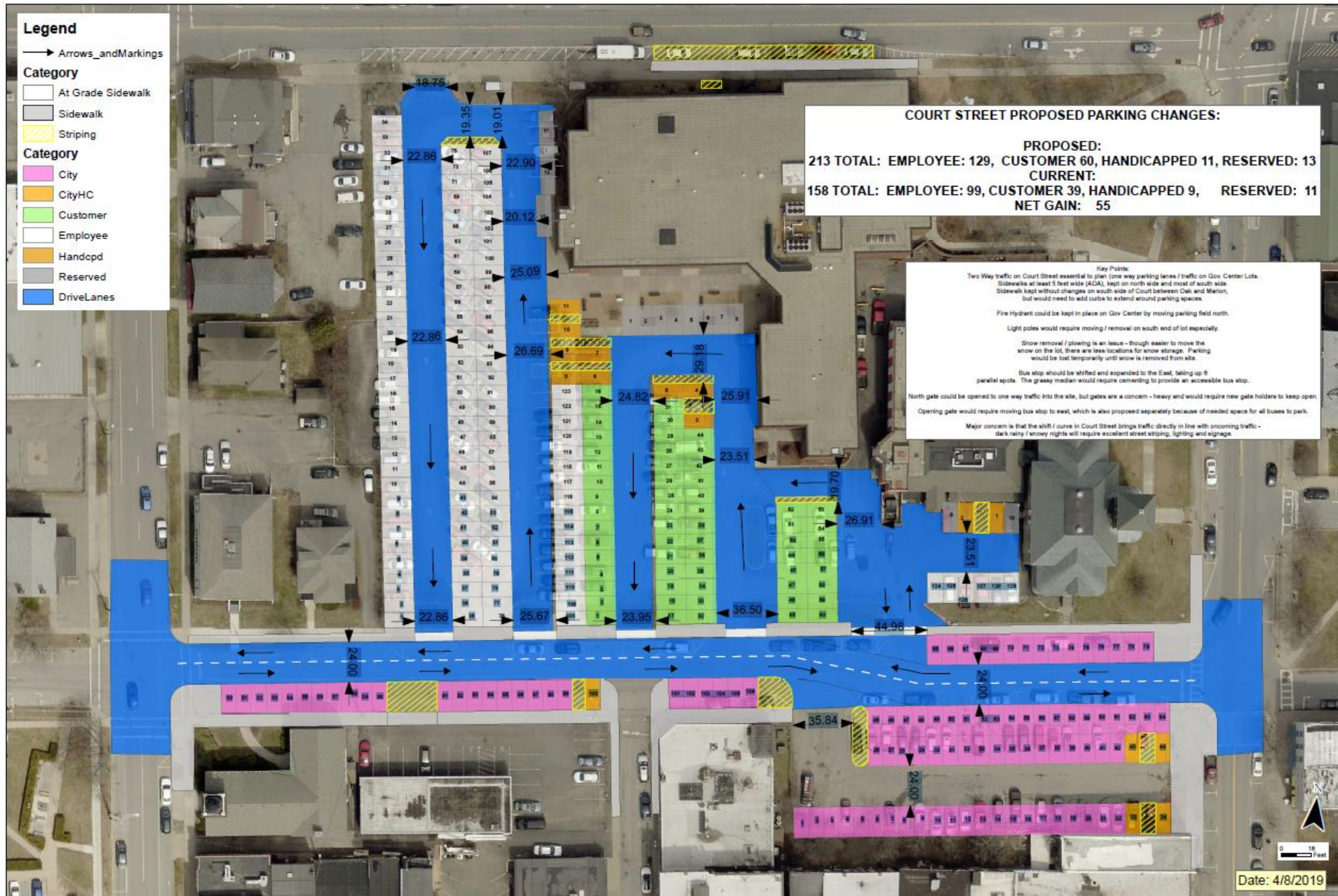
The current model is built with 24 foot driving lanes to allow for the continuation of two-way traffic.

Cost Estimate: \$426,654

Cost Per Newly Constructed Spot: \$12,548

**Legend**

- Arrows\_andMarkings
- Category
- At Grade Sidewalk
- Sidewalk
- Striping
- Category
- City
- CityHC
- Customer
- Employee
- Handcpd
- Reserved
- DriveLanes



**COURT STREET PROPOSED PARKING CHANGES:**

**PROPOSED:**  
 213 TOTAL: EMPLOYEE: 129, CUSTOMER 60, HANDICAPPED 11, RESERVED: 13  
**CURRENT:**  
 158 TOTAL: EMPLOYEE: 99, CUSTOMER 39, HANDICAPPED 9, RESERVED: 11  
**NET GAIN: 55**

*Key Points:*

- Two Way traffic on Court Street essential to plan (one way parking lanes / traffic on Gov Center Lots.
- Sidewalks at least 5 feet wide (ADA), kept on north side and most of south side.
- Sidewalk kept without changes on south side of Court between Oak and Marion, but would need to add curbs to extend around parking spaces.
- Fire Hydrant could be kept in place on Gov Center by moving parking field north.
- Light poles would require moving / removal on south end of lot especially.
- Snow removal / plowing is an issue - though easier to move the snow on the lot, there are less locations for snow storage. Parking would be lost temporarily until snow is removed from site.
- Bus stop should be shifted and expanded to the East, taking up 6 parallel spots. The grassy median would require cementing to provide an accessible bus stop.
- North gate could be opened to one way traffic into the site, but gates are a concern - heavy and would require new gate holders to keep open.
- Opening gate would require moving bus stop to east, which is also proposed separately because of needed space for all buses to park.
- Major concern is that the shift / curve in Court Street brings traffic directly in line with oncoming traffic - dark entry / snowy nights will require excellent street striping, lighting and signage.

Date: 4/8/2019

# Court Street Reconfiguration Option B

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## +14 Spaces

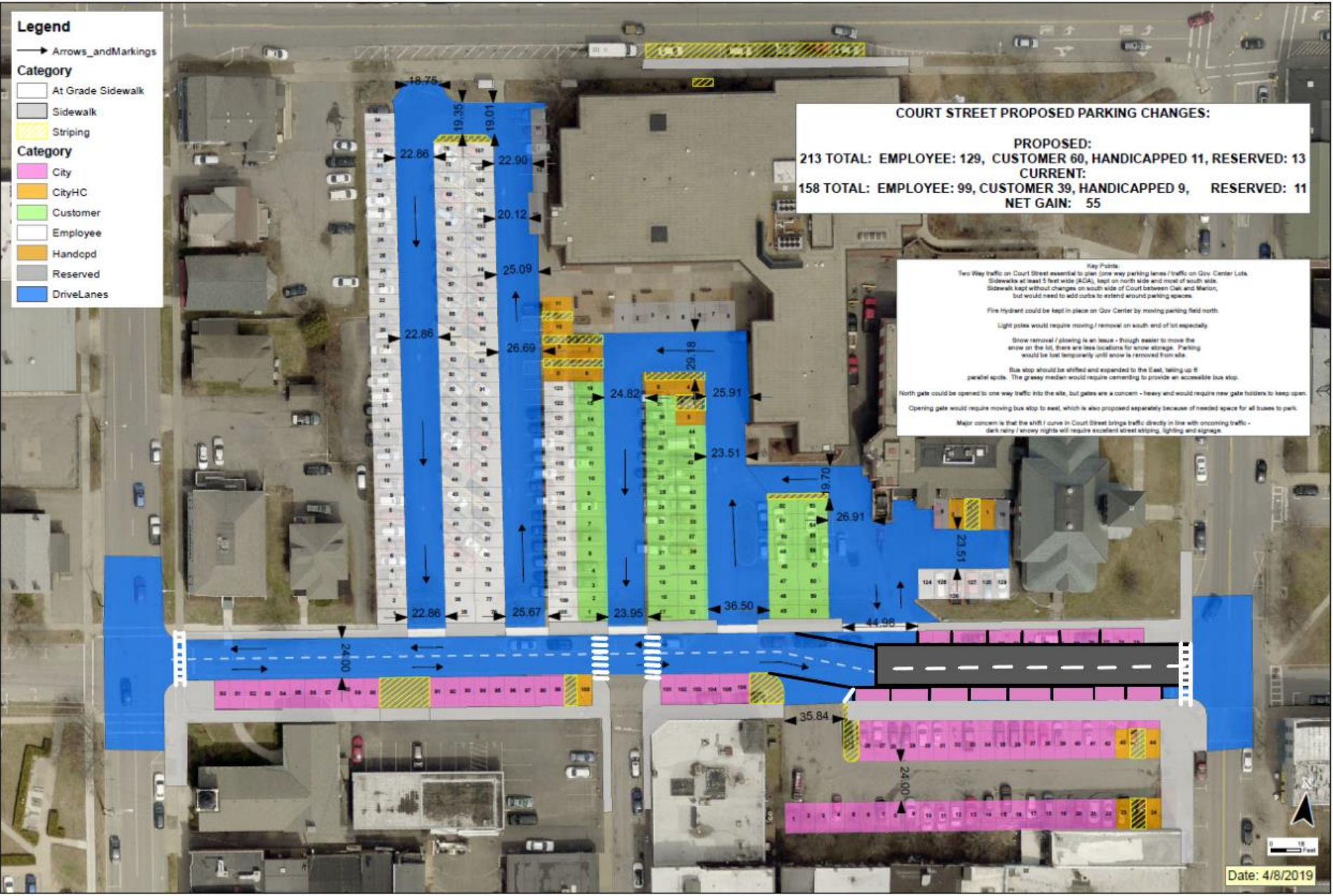
In the model shown earlier this section of Court Street would still behave as a street as it currently does with two-way traffic.

The entrance to Court Street from Margaret Street would remain unchanged and the sidewalks on both sides of the street would be retained. Orthogonal parking would be implemented on the south side of Court Street from Olive Ridley's to Oak Street.

Traffic calming measures could be implemented

- Reduced Speed Limit
- Stop Signs and Pedestrian crossings at Marion Street





# Brinkerhoff Street Reconfiguration

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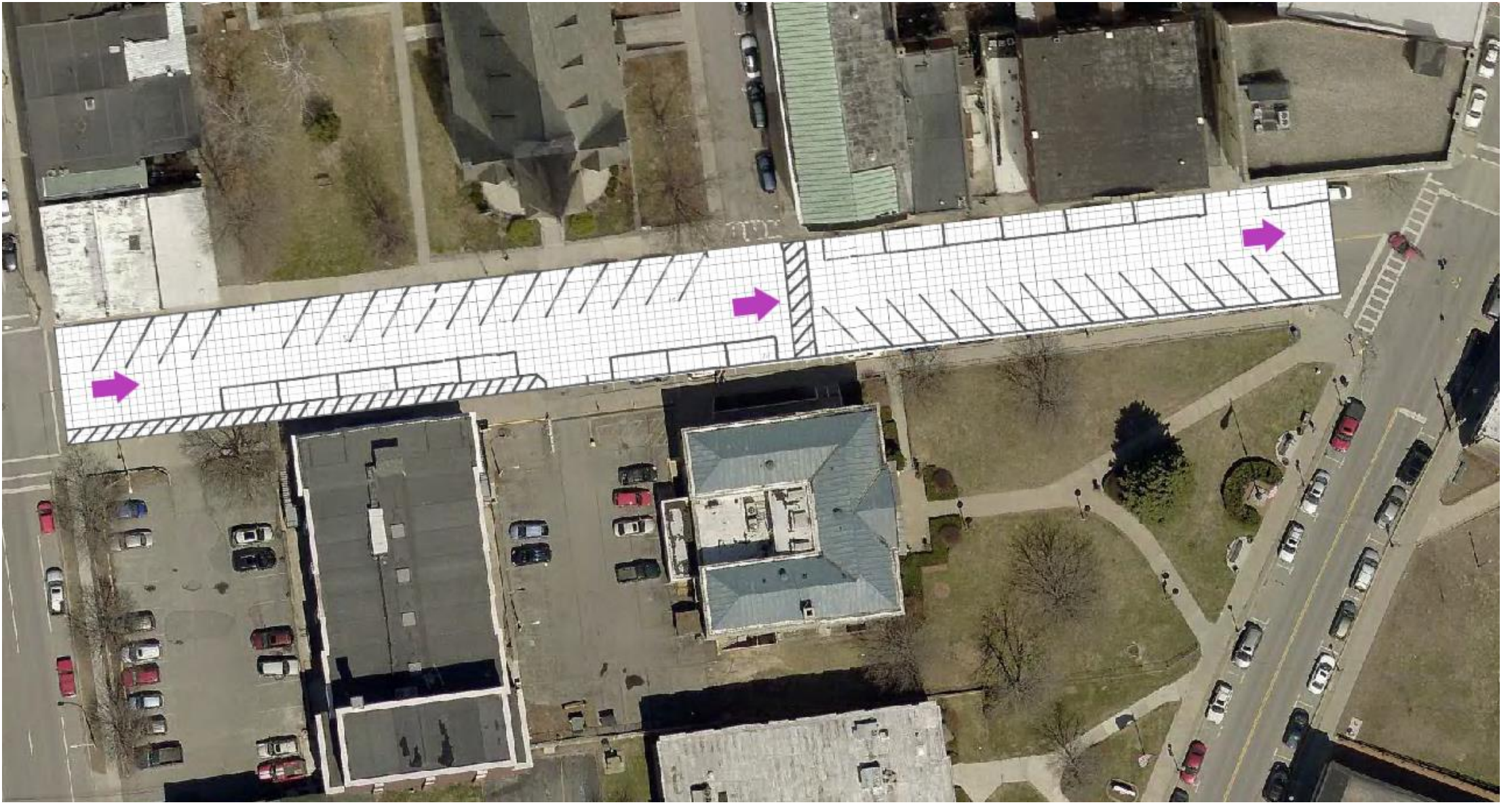
## **+11 Spaces**

Previously discussed, this would turn this section of Brinkerhoff into a one-way, one-lane street.

It would have a combination of parallel parking and 45 degree angled parking.

Driving Lane would fluctuate between 12 feet – 18 feet.

Traffic Calming Measures can be taken if necessary as well.



# Parking Replacement Options

Rank	Option	Net New Spaces (Public)
1	Arnie Pavone Memorial Parking Plaza	115
2	Durkee Development	50
3	County Main Lot Expansion	60
4	Durkee Street - One Way	43
5	Bridge Street	6
6	Broad Street Lot Expansion	15
	<b>Total</b>	<b>289</b>
7	Durkee Street -Two Way	27
8	Court Street Option B	14
9	Brinkerhoff Street Reconfiguration	10
10	Court Street Option A	34